


Notice of Application for a Planning Permit

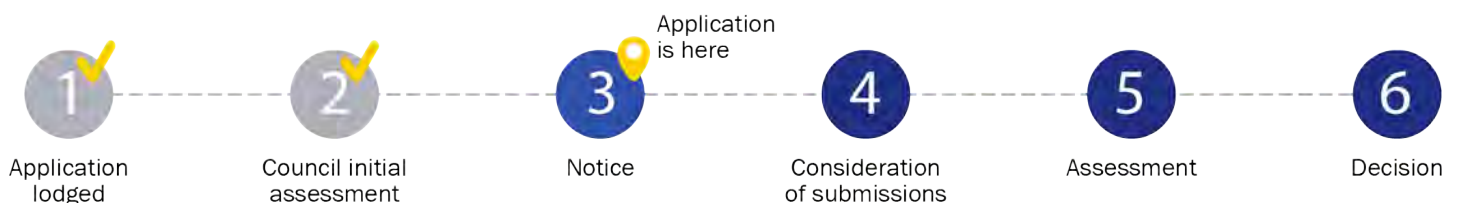
The land affected by the application is located at:	LH PS400130 Princes Highway, Pakenham VIC 3810
The application is for a permit to:	Construction of Twenty-Six Dwellings, Removal of Native Vegetation and Reduction In Car Parking

APPLICATION DETAILS

The applicant for the permit is:	Pakenham Property Holdings Pty Ltd
Application number:	T220321
You may look at the application and any documents that support the application at the office of the Responsible Authority: Cardinia Shire Council, 20 Siding Avenue, Officer 3809. This can be done during office hours and is free of charge. Documents can also be viewed on Council's website: cardinia.vic.gov.au/advertisedplans or by scanning the QR code.	

HOW CAN I MAKE A SUBMISSION?

This application has not been decided. You can still make a submission before a decision has been made. The Responsible Authority will not decide on the application before:	28 June 2024	
WHAT ARE MY OPTIONS? Any person who may be affected by the granting of the permit may object or make other submissions to the responsible authority.	An objection must: <ul style="list-style-type: none">• be made to the Responsible Authority in writing;• include the reasons for the objection; and• state how the objector would be affected. If you object, the Responsible Authority will notify you of the decision when it is issued.	The Responsible Authority must make a copy of every objection available at its office for any person to inspect during office hours free of charge until the end of the period during which an application may be made for review of a decision on the application.



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Application Summary

Portal Reference A2228347

Basic Information

Proposed Use Development of the land for multiple dwellings, removal of vegetation and reduction in the visitor car parking requirements

Current Use vacant

Cost of Works \$4,160,000

Site Address Princes Highway Pakenham 3810

Covenant Disclaimer

Does the proposal breach, in any way, an encumbrance on title such as restrictive covenant, section 173 agreement or other obligation such as an easement or building envelope? Yes, one or more encumbrances are breached

This proposal must include all details of request to change restrictive covenant, section 173 or other obligation to be considered.

Contacts

Type	Name	Address	Contact Details
Applicant	Pakenham Property Holdings Pty Ltd	180-182 Coventry Street, South Melbourne VIC 3205	W: 0400-829-235 M: 0400-829-235 E: phillipr@connectplanning.com.au
Owner	[REDACTED]		
Preferred Contact	[REDACTED] Connect Town Planning	GPO Box 402, Sandringham VIC 3191	W: +61-400-829-235 E: phillipr@connectplanning.com.au

Fees

Regulation Fee Condition	Amount	Modifier	Payable
9 - Class 13 More than \$1,000,000 but not more than \$5,000,000	\$3,464.40	100%	\$3,464.40
Total			\$3,464.40

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Meetings

Meeting Type	Officer Name	Date of Meeting
Pre Application	various - via telephone	11 Feb 2021



Civic Centre
20 Siding Avenue, Officer, Victoria

Council's Operations Centre (Depot)
Purton Road, Pakenham, Victoria



Postal Address
Cardinia Shire Council
P.O. Box 7, Pakenham VIC, 3810

Email: mail@cardinia.vic.gov.au



Monday to Friday 8.30am-5pm
Phone: 1300 787 624
After Hours: 1300 787 624
Fax: 03 5941 3784

Documents Uploaded


Date	Type	Filename
17-05-2022	A Copy of Title	Certificate of Title.pdf
17-05-2022	Encumbrance	S012533S Agreement.pdf
17-05-2022	Encumbrance	T148994L - Agreement.pdf
17-05-2022	Existing floor plan	princes hwy pakenham town plan May 2022.pdf
17-05-2022	Additional Document	Princes Highway Pakenham_Cover Letter Application.pdf
17-05-2022	Additional Document	Princes Highway Pakenham Townhouses_Planning Report - May 2022.pdf
17-05-2022	Additional Document	220184TIA001C-F-All.pdf
17-05-2022	Additional Document	220412 PrInces Hwy Pakenham WMP.pdf
17-05-2022	Additional Document	2205564_RFL.pdf
17-05-2022	Additional Document	22-146AR Lot H Princes Highway Pakenham.pdf
17-05-2022	Additional Document	MPLCERT21265.pdf

Remember it is against the law to provide false or misleading information, which could result in a heavy fine and cancellation of the permit

Lodged By

Site User	 Connect Town Planning	PO BOX 402, Sandringham VIC 3191	W: 0400-829-235 M: 0400-829-235 E: phillipr@connectplanning.com.au
Submission Date	17 May 2022 - 09:10:PM		

Declaration

By ticking this checkbox,  declare that all the information in this application is true and correct; and the Applicant and/or Owner (if not myself) has been notified of the application.

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20 Siding Avenue, Officer, Victoria

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The Victorian Government acknowledges the Traditional Owners of Victoria and pays respects to their ongoing connection to their Country, History and Culture. The Victorian Government extends this respect to their Elders, past, present and emerging.

REGISTER SEARCH STATEMENT (Title Search) Transfer of Land Act 1958

VOLUME 10315 FOLIO 874

Security no : 124114242362X
Produced 17/04/2024 01:19 PM

LAND DESCRIPTION

Lot H on Plan of Subdivision 400130H.
PARENT TITLE Volume 10179 Folio 678
Created by instrument PS400130H 04/02/1997

REGISTERED PROPRIETOR

ENCUMBRANCES, CAVEATS AND NOTICES

Any encumbrances created by Section 98 Transfer of Land Act 1958 or Section 24 Subdivision Act 1988 and any other encumbrances shown or entered on the plan set out under DIAGRAM LOCATION below.

AGREEMENT Section 173 PLANNING AND ENVIRONMENT ACT 1987
S012533S 14/07/1992

DIAGRAM LOCATION

SEE PS400130H FOR FURTHER DETAILS AND BOUNDARIES

ACTIVITY IN THE LAST 125 DAYS

NUMBER	STATUS	DATE
AX896381M (E)	REMOVAL OF AGREEMENT Registered	16/04/2024

-----END OF REGISTER SEARCH STATEMENT-----

Additional information: (not part of the Register Search Statement)

Street Address: PRINCES HIGHWAY PAKENHAM VIC 3810

ADMINISTRATIVE NOTICES

NIL

DOCUMENT END

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Document Type	Plan
Document Identification	PS400130H
Number of Pages (excluding this cover sheet)	2
Document Assembled	12/05/2022 10:47

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PLAN OF SUBDIVISION		STAGE No.	LTO USE ONLY EDITION 1	PLAN NUMBER PS 400130 H
LOCATION OF LAND PARISH: NAR NAR GOON TOWNSHIP: - SECTION: - CROWN ALLOTMENT: 40 (PART) CROWN PORTION: - LTO BASE RECORD: LITHO TITLE REFERENCES: VOL. 10179 FOL. 678 LAST PLAN REFERENCE/S: PS.320966U (LOT A) POSTAL ADDRESS: LOT A PRINCES HIGHWAY (At time of subdivision) PAKENHAM AMG Co-ordinates (of approx centre of land in plan) E 368440 ZONE: 55 N 5785320		COUNCIL CERTIFICATION AND ENDORSEMENT COUNCIL NAME: CARDINIA SHIRE COUNCIL REF: 596/032 1. This plan is certified under Section 6 of the Subdivision Act 1988. 2. This plan is certified under Section 11(7) of the Subdivision Act 1988. Date of original certification under Section 6: / / 3. This is a statement of compliance issued under Section 21 of the Subdivision Act 1988. OPEN SPACE (i) A requirement for public open space under Section 18 of the Subdivision Act 1988 has/has not been made. (ii) The requirement has been satisfied. (iii) The requirement is to be satisfied in Stage Council Delegate Council Seal Date 25 / 7 / 96 Re-certified under Section 11(7) of the Subdivision Act 1988. Council Delegate Council Seal Date / /		
VESTING OF ROADS AND/OR RESERVES				
IDENTIFIER	COUNCIL/BODY/PERSON			
NIL				
NOTATIONS				
STAGING This is/is not a staged subdivision. Planning permit No				
DEPTH LIMITATION DOES NOT APPLY.				
NOTE: LOT A HAS BEEN OMITTED FROM THIS PLAN. TITLE BEARINGS HAVE BEEN ROTATED BY -1°01' FOR AMG DATUM.				
SURVEY THIS PLAN IS/ IS NOT BASED ON SURVEY THIS PLAN HAS BEEN CONNECTED TO PERMANENT MARKS No(s) 54 & 112 IN PROCLAIMED SURVEY AREA No. 72				
EASEMENT INFORMATION				LTO USE ONLY
LEGEND A - Appurtenant Easement E - Encumbering Easement R - Encumbering Easement (Road)				STATEMENT OF COMPLIANCE/ EXEMPTION STATEMENT
				RECEIVED <input checked="" type="checkbox"/>
				DATE 8 / 1 / 97
				LTO USE ONLY
				PLAN REGISTERED
				TIME 4.10
				DATE 4 / 2 / 97
				SHEET 1 OF 2 SHEETS
KLM DEVELOPMENT CONSULTANTS 4/50 THOMAS STREET. DANDENONG, 3175 PH. (03) 9794-9438		LICENSED SURVEYOR (PRINT) ...MICHAEL B. SWALES..... SIGNATURE DATE 26 / 4 / 96 REF 29130/SL VERSION 1		
		DATE 25 / 7 / 196 COUNCIL DELEGATE SIGNATURE ORIGINAL SHEET SIZE A3		

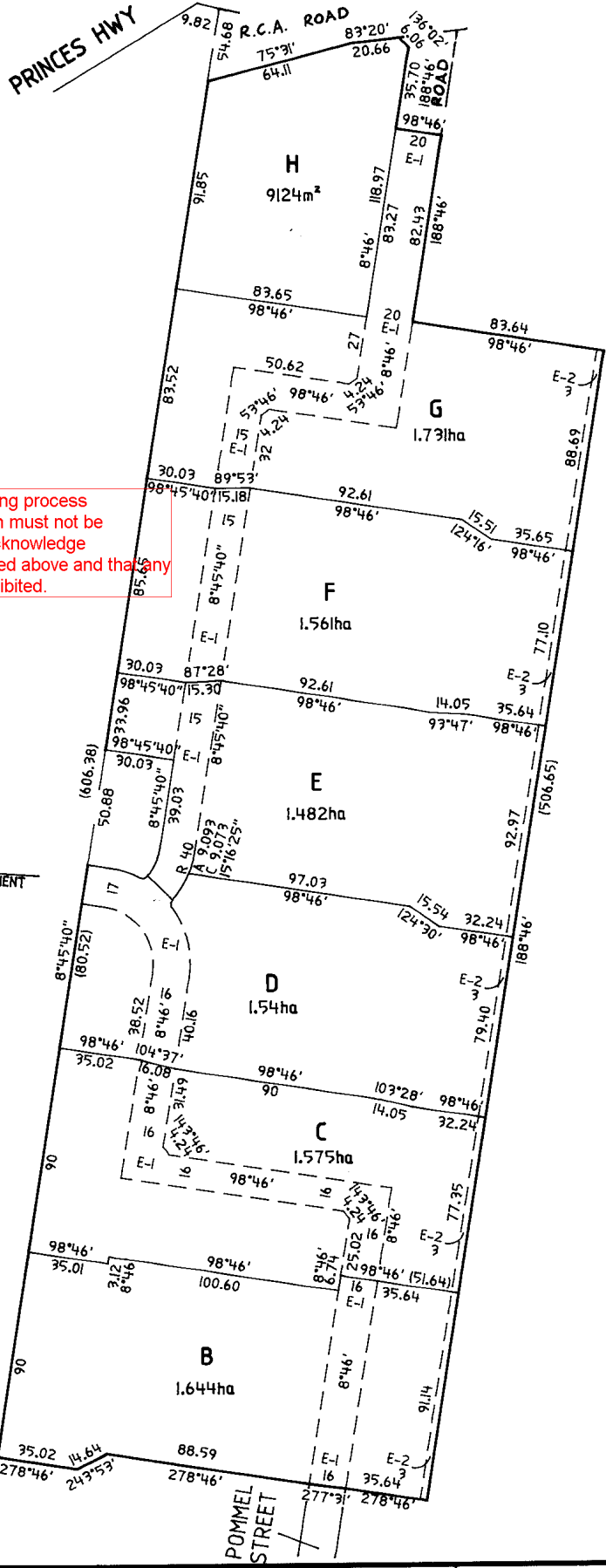
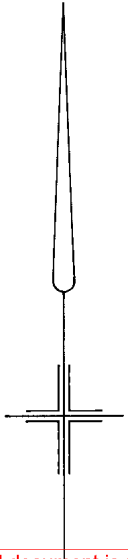
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PLAN OF SUBDIVISION

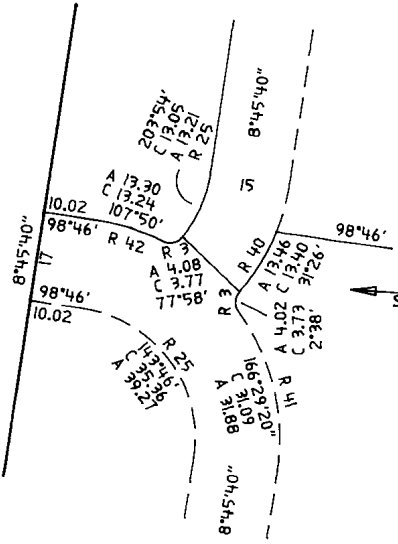
STAGE No.

PLAN NUMBER

PS 400130 H

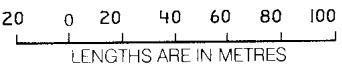


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SEE ENLARGEMENT

KLM DEVELOPMENT CONSULTANTS
 4/50 THOMAS STREET.
 DANDENONG, 3175
 PH. (03) 9794-9438



ORIGINAL SCALE SHEET SIZE
 1:2000 A3

LICENSED SURVEYOR (PRINT) MICHAEL B. SWALES
 SIGNATURE DATE / /
 REF 29130/SL VERSION 1

SHEET 2 OF 2 SHEETS
 DATE 25 / 7 / 96
 COUNCIL DELEGATE SIGNATURE



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140792 1334 11150 454 50125335
50125336

Lodged by
MACPHERSON & KELLEY
416-420 COLLINS STREET, MELBOURNE

11615

ATTENTION: CONTROL/ISSUE ORDER AFFECTS - SEE BACK

2/5/92

APPLICATION BY A RESPONSIBLE
AUTHORITY under Section 181
Planning and Environment Act
1987 for ENTRY OF A
MEMORANDUM OF AGREEMENT
under Section 173 of the Act.

The Responsible Authority under the Planning Scheme having entered into an Agreement with the parties named for the land described requires that a memorandum of the Agreement be entered on the Certificate of Title to the land referred to.

LAND Certificate of Title Volume 9746 Folio 224

ADDRESS OF THE LAND Lot 2 Plan of Subdivision 206134W
Princes Highway Pakenham

RESPONSIBLE AUTHORITY The President Councillors and
Ratepayers of the Shire of Pakenham

PLANNING SCHEME Pakenham Planning Scheme

AGREEMENT DATE December 10, 1990 AGREEMENT WITH
CAIRNDALE PTY. LTD. of P.O. Box 1055 (Suite 2, 50 Thomas
Street) Dandenong, Victoria and COMMONWEALTH BANK OF AUSTRALIA
of 367 Collins Street, Melbourne, Victoria

A copy of the Agreement is attached

Signature for the Responsible Authority

TOWN COUNCIL

Date ... 9/7/92 ...

9/7/92

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HTSC 854 50125335

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AGREEMENT

THE COUNCIL OF THE SHIRE OF PAKENHAM

of the first part

of CAIRNDALE PTY.LTD.

of the second part

and *Commonwealth Bank of Australia*
(the Mortgagee)

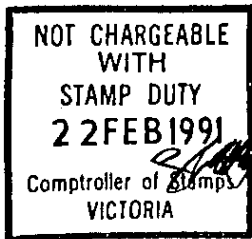
of the third part

I certify that this is a true copy of an agreement pursuant to Section 173 of the Planning & Environment Act, entered into between the President, Councillors & Ratepayers of the Shire of Pakenham, Cairndale P/L and the Commonwealth Bank of Australia on the



THIS AGREEMENT made the 10th day of December 1990

BETWEEN :



THE PRESIDENT COUNCILLORS AND RATEPAYERS OF THE SHIRE OF PAKENHAM
of Henty Way, Pakenham, Victoria
(the Council)

of the first part

of CAIRNDALE PTY.LTD.
of P.O.Box 1055, Dandenong, Victoria
(the Owner)

of the second part

of Commonwealth Bank of Australia
(the Mortgagee)
of 1367 Collins St. Melbourne, Victoria
of the third part

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WHEREAS :

- A. The Owner is or is entitled to be registered at the Land Titles Office as the registered proprietor of Lot 2, on Plan of Subdivision No. 206134W and being the whole of the land comprised in Certificate of Title Volume 9746 Folio 224 ("the Land");
- B. The Council is the Responsible Authority under the Planning and Environment Act 1987 ("the Act") for the administration of the Pakenham Planning Scheme ("the Scheme") and is the Planning Authority pursuant to the Act for the local section of the Scheme;
- C. The Land referred to in recital A. is zoned "Residential 4" under the Pakenham Planning Scheme;
- D. The Owner has made application to the Responsible Authority pursuant to the provisions of the Planning and Environment Act 1987 for the preparation of an amendment (hereinafter referred to as "the Proposed Amendment") to the said Scheme

11034635

to rezone, inter alia, the Land from a Residential 4 zone to part Residential 2 zone and part Highway Facilities 2 zone to allow the subdivision and development of, inter alia, the Land generally in accordance with the tentative plans which accompanied the application and which are annexed hereto as Appendix A ("the plan or plans");

- D. The Council has agreed to prepare an appropriate Amendment to the Scheme subject to the execution of this Agreement and its registration on the Certificate or Certificates of Title to the land.
- E. The Owner has agreed to contribute to the Subdivision Infrastructure Levy
- F. The Council and the Owners have agreed that without restricting or limiting their respective powers to enter into this Agreement and insofar as it can be so treated this Agreement shall be treated as being an Agreement under Section 173 of the Act.

IT IS HEREBY AGREED by and between the parties as follows:

1. **DEFINITIONS**

- 1.1 "The Owner" shall mean the person or persons entitled from time to time to be registered by the Registrar of Titles as the proprietor or proprietors of an estate in fee simple of the Land or any part thereof;
- 1.2 "The Council" is the Shire of Pakenham.
- 1.3 "The Land" means Lot 2, Plan of Subdivision No. 206134W Princes Highway, Pakenham;
- 1.4 "The Contribution" means the moneys payable by the

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Owner to the Responsible Authority set out in Clause 10 hereof.

1.5 "Bank Guarantee" means a guarantee by a bank within the meaning of the Bank Act 1959 in the Commonwealth as amended in force for the time being which carries on business in Victoria or a bank constituted by a law of the State or the Commonwealth;

1.6 "Community Facility Site" means the site or sites to be transferred in title by the Owner to Council for the construction of a community facility marked "A" on the Plan attached hereto;

1.7 "The Development Levy Policy" is the policy document of the Council for the payment of levies applicable to residential subdivision in Pakenham Township.

2. REZONING

2.1 Subject in all respects to the terms and conditions of this agreement and insofar as it is lawfully able to do so, the Council shall use its best endeavours to prepare and place on public exhibition an amendment to the Pakenham Planning Scheme to rezone the Land marked "B" on the Plan attached hereto to a Highway Facilities 2 Zone as defined in the Appendix hereto, the Land marked "C" on the Plan attached hereto to a Residential 2 zone and the land marked "A" on the Plan attached hereto as a Public Purposes 1 Reservation (Municipal Purposes).

3. LAND TO BE USED FOR COMMUNITY FACILITY

3.1 The Owner will, free of expense to the Responsible

Authority and without consideration other than that herein expressed, transfer to the Responsible Authority the Community Facility Site with adequate road frontage constructed to the requirements and satisfaction of the Shire Engineer, before any allotment other than any "Reserve for Municipal Purposes" is transferred. Such transfer shall be prepared and registered by the Responsible Authority's Solicitors at the expense of the Owner, and the Owner shall make available at the Titles Office, Melbourne, the Title or titles to the said Site when called upon to do so after registration of the Plan of Subdivision to enable such transfer to be registered free of all encumbrances save and except drainage and sewerage easements.

4. FURTHER AGREEMENT

4.1 Each of the parties hereto shall respectively and expeditiously sign and execute all further documents and deeds and do all acts and things as the other party shall reasonably require for completely effectuating this Agreement.

4.3 Council agrees that it will not require any additional Public Open Space Contribution other than previously paid to the Council via a cash-in lieu payment relating specifically to the subject site or as provided in the Development Levy Contribution in Clause 10 hereof.

5. COSTS

5.1 Each party shall bear their own costs of and incidental to the making of this Agreement.

5.2 The Owner shall bear any stamp duty payable in respect of this Agreement.

6. DISPUTES/DIFFERENCES/ARBITRATION

6.1 Where any dispute or difference shall arise between the parties (other than in circumstances where statutory procedures must be followed and applied) either party may give to the other notice of intention to refer such dispute or difference to arbitration if such dispute or difference shall not be resolved within 30 days of giving of such notice and if such dispute or difference shall not be so resolved within such period then either party may refer such dispute or difference to the arbitration of the President of the Law Institute of Victoria or his nominee and such arbitration shall be governed by the provisions of the Arbitration Act 1984.

7. GENERAL WARRANTIES

7.1 The obligations imposed upon and assumed by the Owner shall be binding upon and the rights of the Owner shall ensure for the benefit of the Owner's successors, transferees, purchasers, mortgagees, assignees and any person obtaining possession of the whole or part of the Owner's land as if each of such successors had separately executed this Agreement.

7.2 The Owner shall not sell, transfer, dispose of or in any way part with possession of the land (in accordance with normal practice for a development of the type proposed) without first disclosing to its successors the existence and nature of this Agreement.

8. APPLICATION OF DIVISION 2 PART 9 PLANNING AND ENVIRONMENT ACT 1987

8.1 Without limiting the operation or effect which this Agreement otherwise has the parties hereto acknowledge that this Agreement is made pursuant to the provisions of Section 173 of the Planning and Environment Act 1987.

8.2 Upon request so to do by the Council, the Owner shall do all things necessary (including signing any further agreement, acknowledgement or document) to enter a memorandum of this Agreement on the Certificates of Title to the Owner's Land in accordance with the provisions of the Planning and Environment Act 1987.

9. MORTGAGEE'S CONSENT

9.1 The Owner shall on or before the expiration of thirty (30) days from the date hereof obtain the consent in writing of any Mortgagee(s) to the terms and conditions of this Agreement.

9.2 Should such consent not be obtained by the due date or by such later date as may be allowed by the Council then this Agreement shall be voidable at the option of the Council.

10. DEVELOPMENT LEVY CONTRIBUTION

10.1 The Owner shall pay the Council an amount of money in accordance with the Council's Development Levy Policy at the time such amount is paid to the Council the amount specified in the Policy for each residential lot created by any subdivision of the Land approved by the

Council as a Planning Authority in accordance with the Pakenham Planning Scheme prior to the Council (in its role as a Municipal Council) issuing a statement of compliance under Section 21 of the Subdivision of Land Act 1988 that all requirements in relation to the Plan of Subdivision have been complied with and the Council shall not be obliged to issue such statement of compliance or notify the Land Titles Office unless and until the Contribution or a Bank Guarantee for the equivalent amount has been received by it (notwithstanding any other requirements pursuant to the Act or any other legislation having already been complied with) or as otherwise agreed by the Council in writing for such payment which may provide for the staging of the subdivision.

- 10.2 This Agreement shall lapse upon the Responsible Authority acknowledging in writing that-
- (a) the contribution payable by the Owner has been received by it; and
 - (b) the Owner has transferred to the Responsible Authority the Site or Sites referred to in Clause 7.5 hereof.

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IN WITNESS WHEREOF the parties hereto have hereunder set their hands and seals the day and year first hereinbefore mentioned.

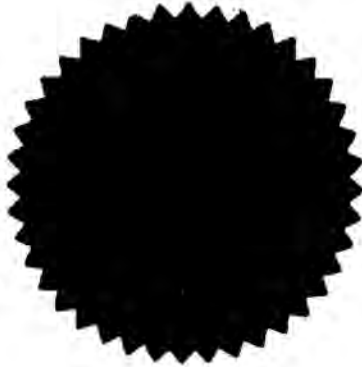


THE COMMON SEAL of THE PRESIDENT COUNCILLORS AND RATEPAYERS OF THE SHIRE OF PAKENHAM was hereunto the presence

Shire Secre

Shire Presi

Councillor:



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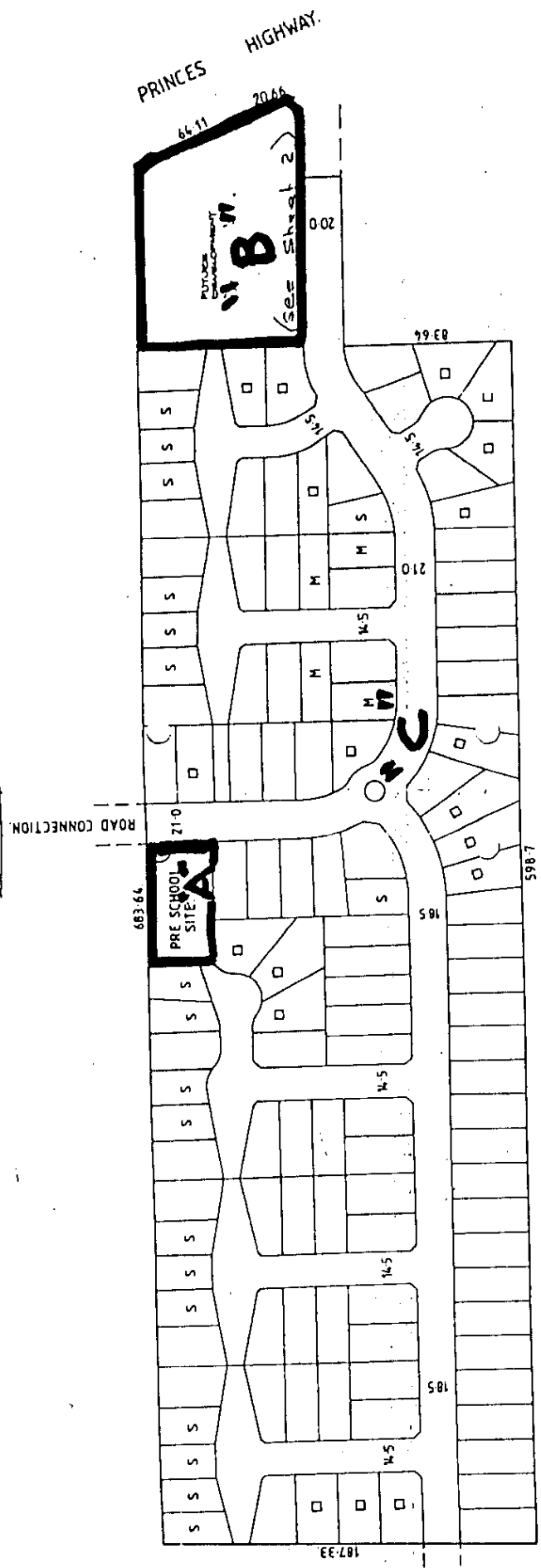
THE COMMON SEAL of _____) (THE MORTGAGEE)
was hereunto affixed in accordance with its articles of association in the presence of:

Director: _____

Secretary: _____



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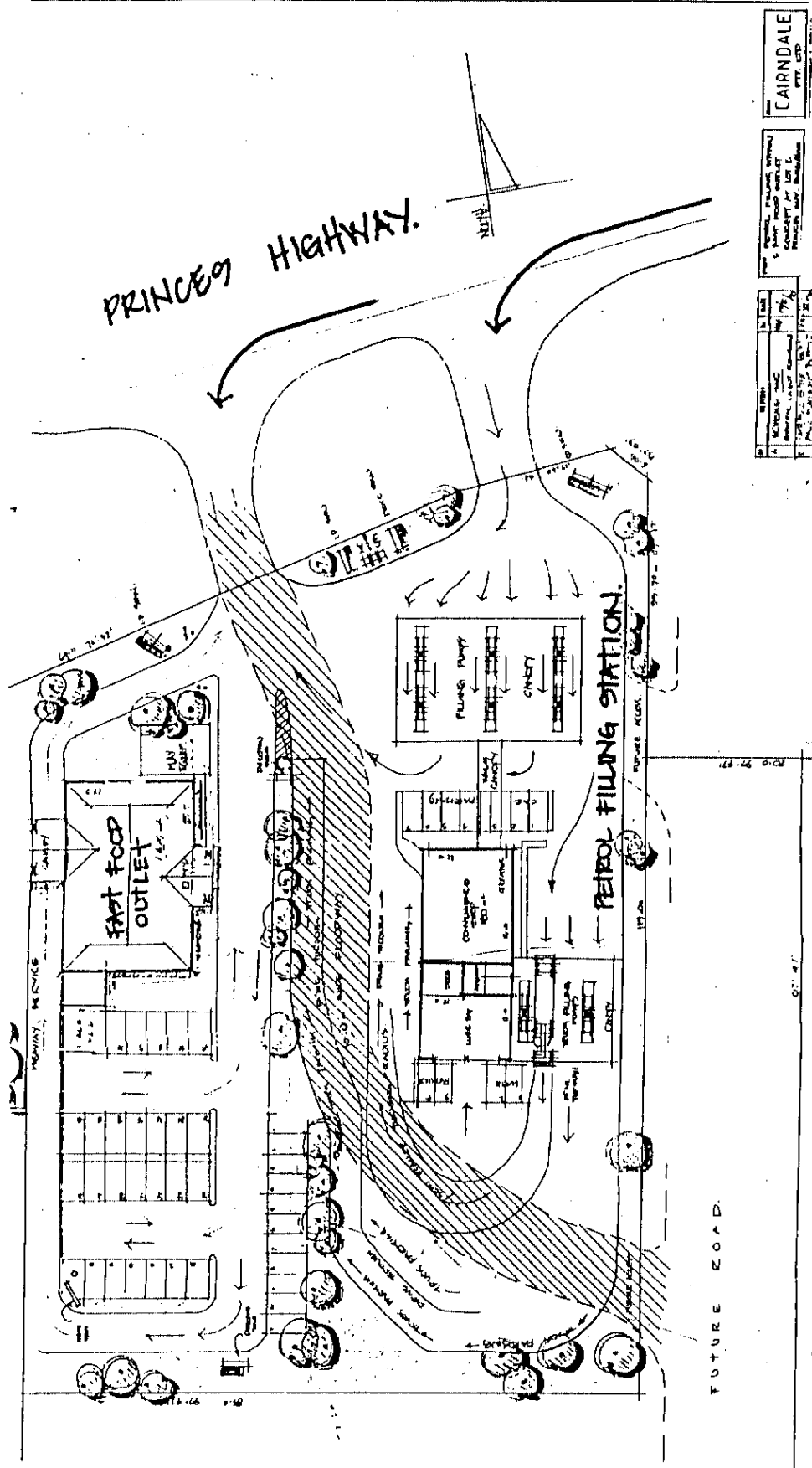


SYMBOL	DESCRIPTION	NO OF ALLOTMENTS
S	ALLOTMENTS BETWEEN 450M ² & 550M ²	19
M	ALLOTMENTS BETWEEN 550M ² & 650M ²	4
	AVERAGE SIZED ALLOTMENT OF 650M ²	83
□	ALLOTMENTS IN EXCESS OF 650M ²	19
	125 TOTAL	

APPENDIX "A" 1 of 2

PROJECT	
INCORPORATED DEVELOPMENT	
LOT 1, LP 200-04	
PACIFIC HWY, PAKENHAM	
SCALE	1:1000
DATE	8/2/90
BY	PAKENHAM

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CAIRNDALE	
MAY 2022	
1. SITE PLAN	7%
2. CONCEPT AT 1:500	
3. PRELIMINARY ENGINEERING	
4. CHECKED BY	
5. APPROVED BY	

Sheet 2

APPENDIX A
2 of 2

SITE LAYOUT PLAN.

PROPOSED RESIDENTIAL SUBDIVISION

Princes Hwy Pakenham Town Planning Report

Prepared for Pakenham Property Holdings Pty Ltd



May 2022

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Connect Town Planning

0400 829 235

phillipr@connectplanning.com.au

www.connectplanning.com.au

PO Box 402 Sandringham Vic 3191

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1. Introduction

The proposal seeks planning approval for construction of twenty six (26) townhouses on land at Princes Highway Pakenham, known as Lot H on PS 400130H.

This report provides:

- A description of the site and surrounding area.
- A description of the proposal.
- An outline of the relevant statutory planning framework.
- An assessment of the proposal against the relevant provisions of the Cardinia Planning Scheme.
- An assessment against Clause 55 (ResCode)

This report should be read in conjunction with the following documents:

- Architectural plans, prepared by Valeo dated May 2022 TP00-TP16.
- Landscape concept plan
- Survey plan, prepared by LandSurveys
- Arborist report by Jonh Patrick Landscape Architects
- Traffic Impact Assessment, prepared by Onemilegrid
- Waste Management Plan, prepared by Leigh Design
- Certificate of title
- Section 173 agreements S012533S & S012533S
- Metropolitan Planning Levy Certificate (MPL)
- Application for planning permit form

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Item	Application Specifics
Address	Princes Highway Pakenham - Lot H on Plan of Subdivision 400130H.
Site Area	9,124sqm overall and 3,980sqm subject to this application
Existing Use and Development	Vacant site - with planning permit in place to use and develop the northern portion of the site for a service station, food and drink premises and 4 shops.
Proposed Use and Development	Development of the land for twenty six (26) townhouse dwellings and reduction of the visitor car parking requirements.
Existing Zone	Mixed Use Zone (MUZ)
Overlay(s)	Development Contributions Plan Overlay Schedule 1 (DCP01)
Planning Policy Framework	11.01 Settlement 16.01-1S Housing 21.01 Cardinia Shire Key Issues & Strategic Vision 21.03 Settlement and Housing 22.04 Highway Development
Particular Provisions	Clause 52.06 Car Parking Clause 55 Two or more Dwellings on a lot
Application Triggers	Pursuant to Clause 32.04-6 a permit is required to construct two or more dwellings on a lot. Pursuant to Clause 52.06-3, a permit is required to reduce the car parking requirements associated with visitor car parking demand. Pursuant to Clause 52.17, a permit is required to remove two (2) native Victorian trees (Tree 2 and 4).
Restrictive Covenants & Easements	Agreement S012533S Agreement T148994L
Title	Lot H on PS400130H

Table 1: Planning Summary Table

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There are no easements which affect the site.

The property is affected by two agreements under s 173 of the Planning & Environment Act including Agreement S012533S and Agreement T148994L.

Agreement S012533S was entered into on 9 July 1992 to facilitate the rezoning of what was the parent subdivision from a Residential 4 zone to part Residential 2 and part Highway Facilities 2 zone and enable the subdivision of the land. The agreement required amongst other matters the vesting of land free of charge to the responsible authority for a community facility.

This agreement has no bearing on the current permit application as the matters required to be satisfied and obligations set out have been addressed and satisfied under the previous rezoning of the land.

Agreement T148994L was created 9 June 1994 under permit P.6095G and includes a number of obligations including that:

3. *Dual Occupancy, split blocks under the Victorian Building Code or flats as described in the Pakenham Planning Scheme will not be permitted on any lot with the exception of lot identical with the lot numbered 148 on the endorsed Permit Plan P6095G dated the 24th May 1993.*

A copy of the endorsed plan under Permit P6095G was not able to be found by Council. Given it is not clear that the agreement exempts the subject site from the restriction of dual occupancy, split blocks or flats, a separate request is being made concurrently to end the agreement from the subject site.

The agreement is considered to be 'dead wood' given its age and the strong policy support provided in the Cardinia Planning Scheme and under the Mixed Use Zoning for increased residential density on this site. It would also appear likely that the site is that identified as Lot 148 in the referenced endorsed plans given it was set aside as a super lot for future more intense development.

Previous Planning Permit T150215

An existing permit is in place for the subject site T150215 enabling the use and development of the northern portion of the site for a service station with associated shop, food and drink premises and 4 shops with a reduction in the loading and unloading requirements of Clause 52.07, variation to Clause 52.12 and access to a Road in a Road Zone 1 and erection of advertising and business identification signage.

The permit was issued on 7 February 2017 and was most recently extended on 18 April 2022 allowing commencement of the development and use by 7 February 2023 and completion by 7 February 2025.

A copy of the plans is provided in **Figure 2** below and shows the approved development. The approval includes the construction of a new service station and kiosk building towards the Princes Highway frontage and development and use of four (4) shops along the eastern boundary facing Sir Thomas Drive.

A new Council standard road at 16m wide is to be constructed to the south of the service station abutting the site of the townhouses proposed under this current application. The future Council road will connect from Sir Thomas Drive through to a new exit point to Princes Highway in the western corner. A new entry point will also be created to Princes Highway to the service station and shops while the current left turn out of Sir Thomas Drive to the Freeway will be changed to an entry only.

Condition 15 of the permit requires this new road to be vested to Council and the existing access to Princes Highway from Sir Thomas Drive is to be closed.

The development has not yet commenced.

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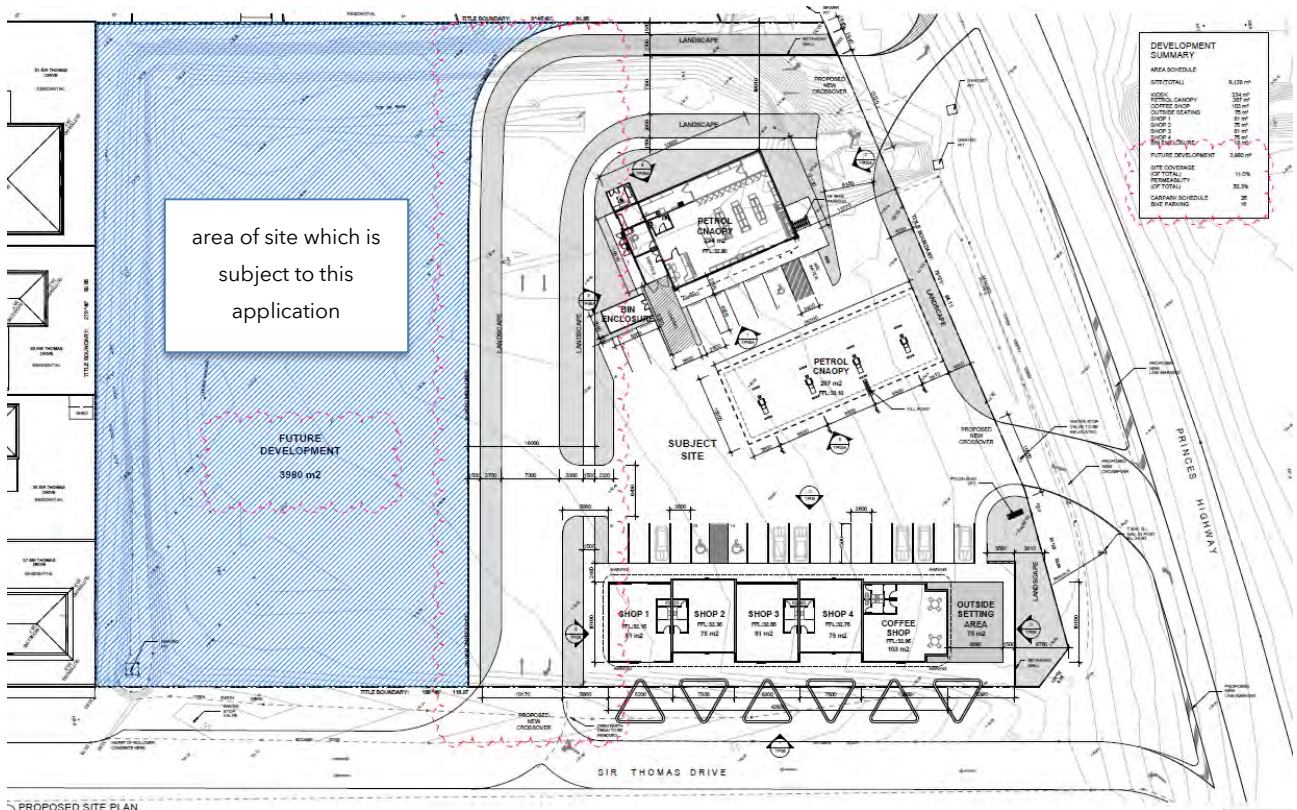


Figure 2: Approved service station and shop development on subject site (T150215).



Figure 3: subject site - view from Sir Thomas Drive looking south-west



Figure 4: subject site - view from Sir Thomas Drive along rear (southern) boundary



Figure 5: subject site - view from within the site looking east towards Sir Thomas Drive



Figure 6: Sir Thomas Drive – looking north towards Princes Highway



Figure 7: Sir Thomas Drive looking south

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3. The Surrounding Area

The subject site is located on a main arterial road (Princes Highway) and is well located in respect to a range of services and facilities including:

- Cardinia Lakes shopping centre including full line Coles supermarket and specialty shops - 350m to the north-east
- Pakenham Place Shopping Centre - 1.8km to the south-west
- Pakenham train station - 2 kmls to the south
- Future Pakenham East train station 2klm to the south-east
- Bus services along Princes Highway
- Pakenham East Primary School - 650m to the north-east
- Pakenham Hills Primary School - 650m to the north-west
- Pakenham Secondary College - 3klm to the west
- Pakenham & District Golf Course - 400m to the east

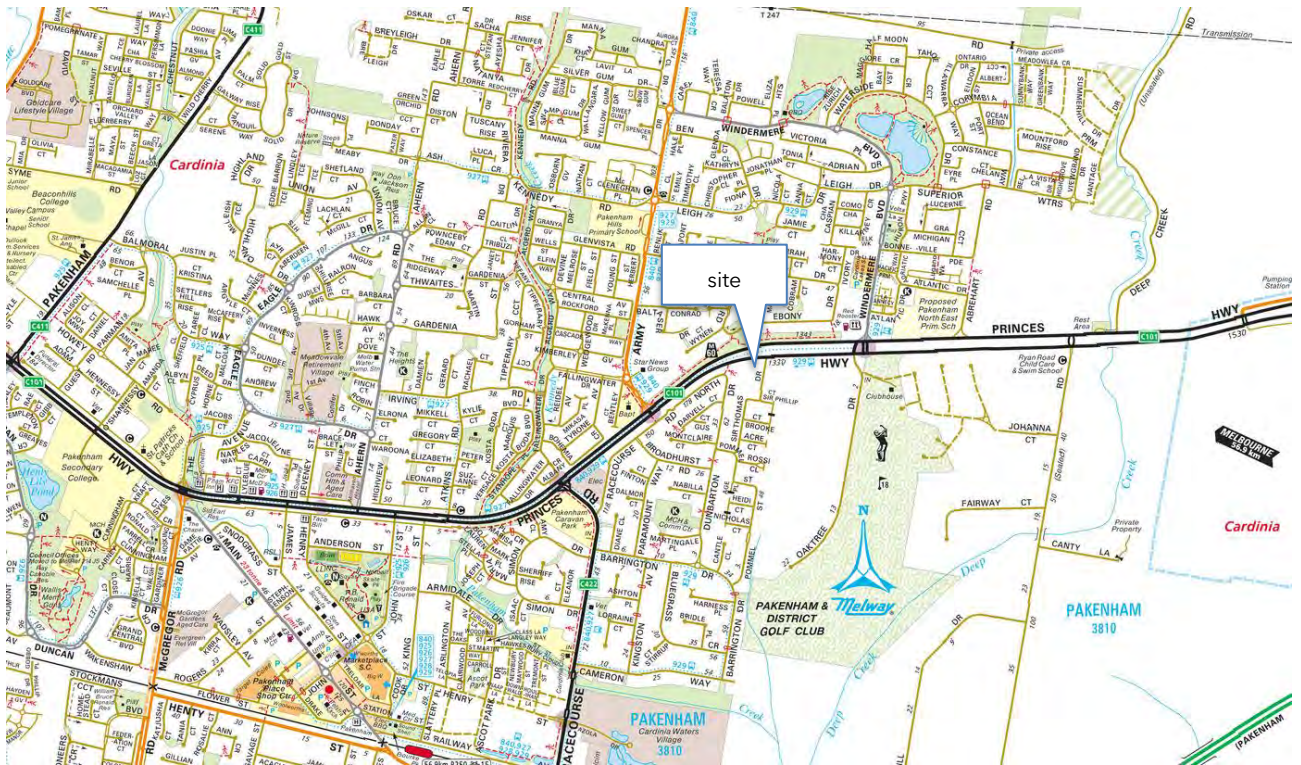


Figure 8: Melways plan - site highlighted

source:www.melways.com

The area surrounding the subject site has been developed for residential dwellings. The surrounding properties consist of single and double storey dwellings set on a range of lot sizes.

Directly adjoining properties include:

- The **eastern** side of Sir Thomas Drive adjacent to the subject site has been developed with a range of multi-unit developments including 11 single storey units at 30 Sir Thomas Drive and 15 single storey villa units at 34 Sir Thomas Drive. The units are constructed in a semi-detached nature and accessed via a central common property laneway/driveway. 32 Sir Thomas Drive is a relatively large block containing a large single storey home.

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Figure 9: Unit development at 30 Sr Thomas Drive



Figure 10: Units at 30 Sir Thomas Drive



Figure 11: Units at 34 Sir Thomas Drive



Figure 12: 32 Sir Thomas Drive

- To the **west** the subject site abuts three residential properties which have a frontage to Dunbar Drive. A 1.8m-2.0m high timber paling fence is constructed along the common boundary. No. 2/82 Dunbar Drive is a single storey brick dwelling which is setback 3.2m from the common boundary. There is an existing covered outbuilding structure located on the boundary and there are four windows facing the subject site. 84 Dunbar Drive is also a single storey brick dwelling which is setback over 14m from the common boundary. 88 Dunbar Drive is a single storey brick dwelling which is setback over 10m from the site.
 - To the **south**, the site abuts the rear boundary of four lots which face south to Sir Thomas Drive as it doglegs. 31-37 Sir Thomas Drive are all developed with single storey brick dwellings with hipped concrete tiled roofs. Their respective backyards abut the subject site and the houses are setback between 3.5m-9.5m from the boundary with the subject site.
- 37 Sir Thomas Drive has its side to the east at a setback of 3.4m with a high timber paling fence constructed along this boundary.

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Figure 13: Aerial photo of subject site and interface with 31-37 Sir Thomas Drive



Figure 14: 37 Sir Thomas Drive - east boundary interface

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4. The Proposal

Dwellings

The proposal involves the construction of twenty six (26) two storey three bedroom townhouses to be constructed in two rows either side of a central common property driveway. The townhouses will be constructed in a north-south orientation within a rear loaded and reverse living arrangement.

Each dwelling will include a double garage, bedroom with ensuite and bathroom, separate powder room and front entry at ground level. First floor level will include the living, dining and kitchen areas facing onto a 12sqm balcony and two additional bedrooms.

Townhouses 1-13 will be constructed on the southern side of the site with a setback of 4m proposed to Sir Thomas Drive and Townhouses 14-26 will face north adjacent to and overlooking the future Council road that will be constructed under the service station permit. The end units facing Sir Thomas Drive are designed with front entries and doors facing the street to provide a suitable degree of activation and engagement with the street.

The two other end townhouses (13 and 14) are setback 3.0m from the western boundary.

At ground level, each townhouse (other than 1 and 26) will include the master bedroom facing the street with a large balcony setback 5m to the street frontage. The master bedroom will include ensuite and walk in robe. Each dwelling will include laundry, powder room, kitchen, meals and family room at this level opening to a large area of secluded private open space including decking at the rear of the dwelling.

Townhouses 1 and 26 are designed with ground level living, dining and open space and face Sir Thomas Drive. First floor level will include three bedroom.

The proposed building design adopts a contemporary coastal architectural response to the site's context.

Site coverage is 60% of the total site area, permeability is 20%.



Figure 15 Perspective of proposal view from Sir Thomas Drive (south)

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Figure 16 Perspective of proposal view from north-west corner



Figure 17 Perspective of proposal along southern townhouses

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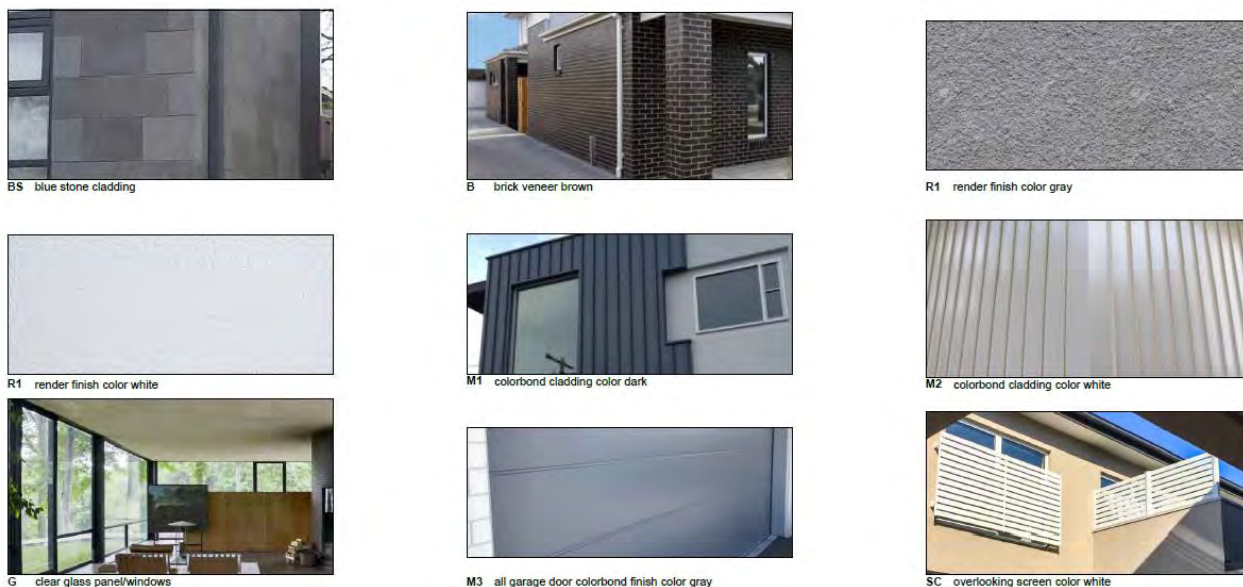


Figure 18 Colour and material palette

A selection of materials and finishes has been chosen, drawing reference from the variety of materials existing in the surrounding area including the use of black charred timber cladding, second hand pressed red bricks and rendered finishes. The roof is to be flat to keep the overall building profile lower in the site.

The proposal will include the removal of existing vegetation from the site. An arborist report has been prepared by John Patrick Landscape Architects which has assessed all existing vegetation on the site. The report confirms that the majority of trees are recent regrowth that has occurred within the last 10 years and large portions of the site are covered by blackberry and weeds.

There are two (2) trees that have been identified as being native (to Victoria) and likely to be more than 10 years old and therefore trigger a permit for removal under Clause 52.17. These are Trees 2 and 4 which are located towards the southern section of the site.

Tree 2 is a Sallow Wattle which a height of 6m and canopy of 4m and has been attributed a low arboricultural value

Tree 4 is a Sallow Wattle which a height of 5.5m and canopy of 4m and has been attributed a low arboricultural value

All other trees are identified as being either not native to Victoria or recent regrowth and all are nominated with a low to medium retention value.

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5. Permit Triggers

The following permit triggers are applicable to the proposal.

5.1 Zoning

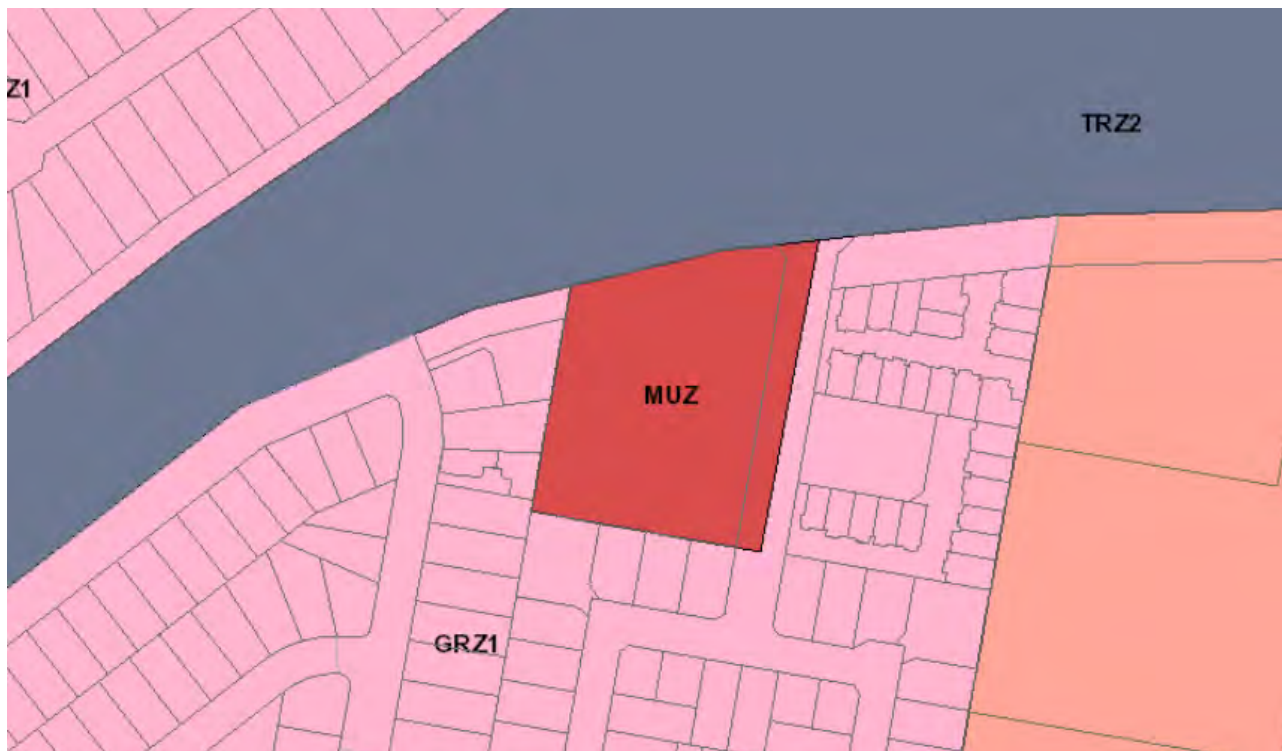


Figure 19 Zoning (Mixed Use Zone)

The subject site is located within the **Mixed Use Zone (MUZ)**.

Under the Mixed Use Zone, a planning permit is not required for the use of the land for a dwelling.

A planning permit is required to construct or carry out buildings and works for two or more dwellings on a lot pursuant to Clause 32.04-06.

5.2 Overlays

5.2.1 Development Contributions Plan Overlay Schedule 1 (DCP01).

The site is located in the Development Contributions Plan Overlay Schedule 1 (DCP01). A development levy will be payable and will be included as a condition on permit.

5.3 Particular Provisions

5.3.1 Car Parking (Clause 52.06)

Clause 52.06 specifies a requirement to provide two (2) car parking spaces for each 3 or more bedroom dwelling.

The proposal satisfies this requirement in that it provides a two car parking spaces for each for each dwelling.

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The proposed garage and access dimensions fully comply with Clause 52.06.

The proposal provides two (2) on-site visitor car parking spaces and there is therefore a shortfall from the five (5) spaces required under this clause. A permit is required to reduce the car parking requirements.

5.3.2 Clause 52.17 Native Vegetation

Pursuant to Clause 52.17. a permit is required to remove two trees (Tree 2 and 4) identified in the arborist report as being native vegetation that is more than 10 years old and triggers an approval under this clause.

5.3.3 Clause 55

Pursuant to Clause 55, an application to construct or extend two or more dwellings on a lot must meet all requirements of Clause 55.

A detailed assessment against all of the objectives and standards of Clause 55 (ResCode) is provided in the Appendix to this report.

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6. Planning Assessment

6.1 Planning Policy Framework

The following clauses of the Planning Policy Framework contained within the Cardinia Planning Scheme are applicable to the application.

Clause 11.01-1 Settlement seeks to promote the sustainable growth and development of Victoria and deliver choice and opportunity for all Victorians through a network of settlements.

Clause 11.02-1S Supply of Urban Land seeks to ensure that there is a supply of land available for residential, commercial, retail, industrial, institutional and other community uses.

Clause 15.01-1S Urban Design includes an objective to create safe, functional and good quality environments with a sense of place and cultural identity.

Clause 15.01-5S - Neighbourhood Character aims to recognise, support and protect neighbourhood character, cultural identity, and sense of place.

Clause 16.01-1S Housing Supply states planning should provide for housing diversity and ensure the efficient provision of supporting infrastructure. New housing should have access to services and be planned for long term sustainability, including walkability to activity centres, public transport, schools and open space.

Clause 16.01-1R Housing Supply Metropolitan Melbourne seeks to manage the supply of new housing to meet population growth and create a sustainable city by developing housing and missed use development opportunities in locations that are areas for residential growth and near existing and proposed train stations.

Clause 16.01-1S Housing Affordability states as the objective to deliver more affordable housing with strategies to improve affordability including increasing choice in housing type, tenure and cost to meet needs of households as they move through life cycle changes to the support diverse communities.

Clause 21.01 Cardinia Shire Key Issues and Strategic Vision notes that The majority of the Cardinia Shire's population is located within the urban areas of Beaconsfield, Officer and Pakenham. These areas are within the Casey-Cardinia South-east Growth Corridor, and will accommodate the majority of future residential and commercial growth. The remainder of the population is located within Cardinia's townships and rural residential areas.

The population within the Cardinia growth area is expected to grow 82% from a current population of around 109,000 people (2018) to approximately 198,000 people in 2041.

Key issues identified in this clause include the sustainable development of the Urban Established Areas, Urban Growth Area, and rural townships and the provision of appropriate rural residential and rural living development.

The subject site is located within a Residential Area as identified on the Cardinia Shire Strategic Framework Plan.

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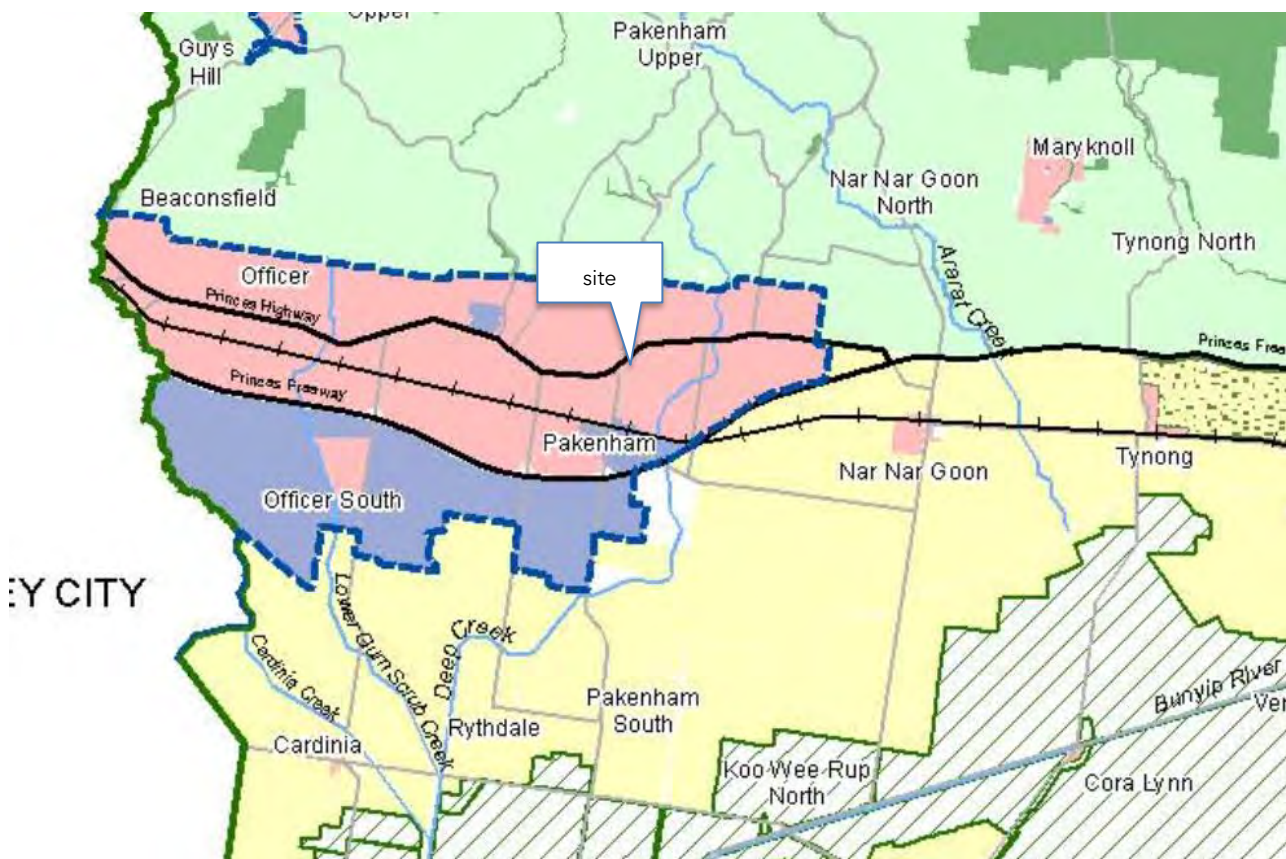


Figure 20: Cardinia Shire Strategic Framework Plan (site highlighted)

Clause 21.03 Settlement and Housing is applicable. The overview notes that housing within the Cardinia Shire is currently dominated by detached dwellings in both urban and rural areas.

The urban area in the Cardinia Shire will continue to attract predominantly young families into the foreseeable future. However, as the housing market progressively matures and the needs of households change there will be increasing demand more diverse forms of housing.

Key issues include:

- Providing for a diversity of housing types and densities, including increased housing density around activity centres.
- Recognising the need for affordability and availability of housing choice for different income levels in both the rental and purchaser markets.
- Recognising the demand for specialist design housing including housing for the aged, people with a disability or mobility issues.
- Identifying the demand for social and community housing.
- Providing opportunity for development of 'Affordable Housing'.

Relevant objectives and strategies include:

Objective 1: To encourage a diversity in housing to meet the needs of existing and future residents.

- Ensure that future housing growth is effectively managed to maintain and enhance the qualities of the municipality.

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- *Encourage a range of lot sizes and housing types in new developments that satisfy the needs and aspirations of the community.*
- *Encourage an increase in densities that has a high standard of urban design, timely provision of infrastructure and is consistent with the existing and/or preferred neighbourhood character.*
- *Encourage medium density housing to be located within or at the interface of activity centres and overlooking local and linear open space.*
- *Ensure residential development and subdivisions are designed to be responsive to existing urban character.*
- *Ensure that new development is located within a safe, attractive and well planned environment that allows residents to maximise opportunities to undertake physical activity.*

Objective 2: To encourage the provision of housing to cater for groups within the community with specific housing requirements.

- *Encourage the development of a diverse, flexible, adaptable range of housing types and tenures.*
- *Encourage development that supports adaptive housing, in appropriate locations.*

Clause 21.03-2 relates to **urban established areas including Beaconsfield and Pakenham** which acknowledges the established urban area located in the Casey-Cardinia Growth Area. Key principles for development include:

- *To ensure greater diversity of housing types and size.*
- *To achieve good urban design outcomes with a strong urban character.*

Clause 22.04 Highway Development has some relevance to the application given the site abuts Princes Highway. Relevant objectives include:

- *To ensure the safe and efficient movement of traffic along highways in the municipality.*
- *To provide for facilities to meet the needs of people travelling along highways.*
- *To protect the rural outlook and visual amenity of highways in rural areas, and the visual amenity of highways in urban areas.*
- *To ensure a high standard of design associated with developments along highways.*

Response to the Planning Policy Framework

The policies outlined above enable planning at a state and municipal level and guide the formulation of local policies to implement positive planning outcomes.

Clause 11 - Settlement

The proposed development is consistent with the requirements of Clause 11. The subject site is in an established urban area.

Clause 15 - Built Environment and Heritage

The proposed two storey scale of the development is respectful of the existing scale of abutting and nearby properties with appropriate consideration given to the design of the townhouses to reflect a contemporary architectural appropriate in this context.

Clause 16 - Housing

The proposal supports the objective of Clause 16 by increasing housing provision in an existing urban area and providing a diversity of new housing choice.

Clause 21.01 - Key Issues and Strategic Vision

The proposal responds to this clause by providing much needed housing diversity at an increased density to meet the substantial expected increase in population that is envisaged in this clause. The site is located in an established residential area and is in good proximity to a range of services and facilities.

Clause 21.03 Settlement and Housing

This clause notes that the majority of housing in the Shire consists of detached dwellings and calls for new development to provide for a greater diversity of dwelling types and densities to meet changing housing needs. The proposal directly responds to this policy issue by delivering a range of houses in a reverse living arrangement and with a layout that provides a point of difference to the predominate housing types in the immediate area. The

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houses, while being three bedroom, are provided on a small lot reducing the need for maintenance and provided at a price that will be affordable when compared to that of the larger conventional lots and houses in the area.

The townhouses are designed to be appropriately sited and setback from adjoining properties to avoid unreasonable impacts of visual bulk and use a mix of materials that are characteristic of the area.

Clause 22.04 Highway Development

The proposed townhouses will provide an appropriate interface with, and not negatively impact, on the operation and development of the approved future service station and shops on the northern portion of the site. The uses will be separated by a 16m wide Council road which will act as a suitable separation buffer to ensure no reverse amenity outcomes are experienced.

On the basis of the above considerations, the proposal is deemed to meet the relevant state and local policies and the site is clearly a suitable candidate for an increased density and diversity of housing.

6.2 Response to Zone

The purpose of the Mixed Use Zone includes:

- *To provide for a range of residential, commercial, industrial and other uses which complement the mixed-use function of the locality.*
- *To provide for housing at higher densities.*
- *To encourage development that responds to the existing or preferred neighbourhood character of the area.*
- *To facilitate the use, development and redevelopment of land in accordance with the objectives specified in a schedule to this zone.*

Relevant decision guidelines under the MUZ include the objectives and standards of Clause 55.

Response to the Mixed Use Zone

The proposed development of the land for a small group of townhouses is considered to be consistent with the purpose of the mixed use zone noting that one of the main purposes is to encourage housing at higher densities.

The dwellings will provide additional housing diversity by offering two storey dwellings within an area that is predominately single storey and at a density that is higher than that of the General Residential land to the east, south and west of the site and in a configuration that provides diversity of housing choice including small, low maintenance houses that will be provided at a price point that is more affordable to that of the larger conventional lots and houses nearby.

Please refer to Appendix A for a response detailing how the proposal responds to the objectives, standards and decision guidelines of Clause 55.

6.3 Built Form and Neighbourhood Character

The proposed extent of built form, massing and setbacks are suitable in this location nominated for increased housing density. The site is located in an established residential area and on land that is zoned Mixed Use Zone. Adjoining development includes larger lots and houses through to smaller unit developments immediately to the east.

Response to the Neighbourhood Character

Other key considerations from a neighbourhood character perspective include:

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Front Setbacks

The proposal includes a setback of Townhouses 1 and 26 of 4m to Sir Thomas Drive which provides a suitable built form transition and integration with the existing 4m setback of the adjoining property at 37 Sir Thomas Drive and the 3m setback associated with the approved shops at the northern section of the site under the service station permit.

Dwelling configuration

The configuration of the dwellings in two rows each facing north and south is a logical and efficient layout for the site, making the most of the property to deliver housing density and diversity. Dwellings on the northern side of the lot will face onto the future Council road to be constructed under the service station and will include an open front yard and entry providing a high degree of surveillance. The townhouses are designed with setbacks of 5m to the future street which will include a 1.5m footpath and 2.5m of landscaping.

The townhouses facing east to Sir Thomas Drive are designed as 'end unit's to face the street and avoid a side of house presentation to the main street. Each of the two townhouses (1 and 26) are provided with a ground level entry facing the street and high levels of articulation and clear glazing.

The south facing townhouses will face a landscaped pedestrian corridor which will run along the southern boundary of the site with dwellings setback 5m to the boundary. This 5m setback will provide for a 1.5m wide footpath and 2.5m of landscaping beds and front garden area ensuring an appropriate setback transition is achieved to the adjoining properties to the south.

The use of reverse living arrangement to the townhouses (other than TH1 and TH26) creates a highly active façade and provides a configuration of housing that is otherwise not available in the immediate surrounding area.



Figure 21 Perspective of proposal view from Sir Thomas Drive (north)

Access

The access way will provide all vehicle access to and from the site and will be broken up with landscaping beds between each garage. Passive surveillance of the laneway is achieved through the use of windows to bedrooms.

Other neighbourhood character considerations

- The townhouses are adequately setback from boundaries to not only allow for new planting but to also assist in providing a sense of spaciousness.
- The proposed materials are consistent and characteristic of materials found in the surrounding area.
- The design includes the use of varied setbacks at the upper levels to avoid blank walls addressing the streets
- The overall building mass is responsive to the size of the site and will sit comfortably in its setting.
- The site coverage is 60%
- Only one crossover is proposed which limits impacts of multiple crossings and increases the opportunities for landscaping in the front setbacks and along the street frontages.
- The overall building mass is responsive to the size of the site and will sit comfortably in its setting providing a two storey built form that is a suitable transition in scale to adjoining single storey dwellings.
- Suitable areas of landscaping are provided within the front and side setbacks and at the end of the driveway to reduce the perception of hard paving and provide a high degree of planting and landscaping opportunities.

In consideration of the design of the dwellings which reflect the character of surrounding properties, the development is considered to appropriately respond to neighbourhood character.

6.4 Amenity Considerations

The proposal is subject to the provisions of Clause 55 in respect to amenity impacts both internal and external.

Response to the Amenity Considerations - External & External

The development is subject to the requirements of Clause 55 (ResCode) of the Planning Scheme for the construction of two or more dwellings on a lot. A full assessment of the proposal against the relevant ResCode provisions is undertaken at Appendix 1.

In respect to **internal amenity**, the following summary is provided:

- The internal living spaces are well proportioned and functional and all living spaces have direct access to an external deck and private open garden space;
- A range of dwelling configurations have been provided with a focus to provide for north facing windows to living areas and balconies wherever possible to maximise opportunities for sunlight and daylight.
- Balconies are provided which have suitable dimensions and aspects to meet the reasonable recreational needs of future residents. The size of the balconies are generous and greater than the minimum requirements under Clause 55.
- Individual mail boxes are provided in the frontage of each dwelling facing the public road and proposed footpaths
- Appropriate provision has been made for storage
- The rooms are generous and are provided with windows achieving adequate daylight

In respect to **external amenity** considerations:

- All side and rear setbacks comply with the standards of ResCode including B17 and in all cases further than the minimum requirements.
- Visual bulk impacts are mitigated through the use of appropriate setbacks over 5m to the south and 3m to the west and with the use of highly articulated façade designs.
- No new walls are proposed to be constructed on the boundary of the site.
- Overlooking into adjoining properties has been avoided with the use of appropriate detailed screening measures. Views to the south at first floor level have been addressed with the use of screens to be applied to

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the edges of the balconies to 1.7m above finished floor level. This limits overlooking but enables the living areas of the townhouses to have clear glazing looking towards the open space.

- Overshadowing of open space is limited to be in accordance with Standard B21.

6.5 Car parking and access

An owners corporation internal laneway is proposed to provide vehicular access to the townhouses to avoid the dominance of crossovers to the adjoining public roads.

Each of the three bedroom dwellings is provided a double garage in accordance with Clause 52.06 of the Planning Scheme.

There are two (2) visitor car parking spaces provided on site which will be set aside for visitors. The application therefore seeks a reduction in the visitor car parking requirements noting that the proposal requires five (5) spaces to be provided. The shortfall of three (3) spaces is considered appropriate to reduce in this instance given that there is ample opportunity for on-street car parking along the frontage of the property which can readily accommodate the identified shortfall.

A traffic impact assessment has been by Onemilegrid and is attached to this application. We refer to the report for further assessment of the proposed car parking and access arrangements including details of the proposed traffic generation.

6.6 Waste Management

Waste storage and collection points have been provided for each row of townhouses in the form of communal bin stores located along Sir Thomas Drive. The bin area will store all bins for the townhouses which will then be collected by private waste contractor. Bin collection will occur from Sir Thomas Drive with waste trucks stopping on the kerb side while being wheeled out to the truck and emptied by the contractor. The bins will then be returned to the communal bin areas.

A detailed Waste Management Plan prepared by Leigh Design is attached to this application.

6.7 Removal of Vegetation

The proposal will include the removal of all vegetation from the site. The John Patrick arborist report notes that the majority of vegetation on the site is of fair to good health and mostly of low arboricultural value. Only two trees trigger a permit under Clause 52.17 for removal being Trees 2 and 4. All other trees and shrubs do not trigger approval and are mostly small recent regrowth with little to now arboricultural or landscape value.

Trees 2 and 4 are located towards the southern boundary of the site and are identified as being Sallow Wattle regrowth that appear to be more than 10 years old and are 5.5m and 6m in height. The trees will be impacted by Townhouses 2 and 4 and are proposed to be removed.

Clause 52.17 includes a provision to avoid, minimise and off-set via the three step approach. In this instance, The arboricultural value of these trees has been identified as being low. Given that these trees are relatively low value and do not necessarily contribute significantly to the landscape or amenity value of the site and as two individual small trees which do not form part of a wider or larger patch of native vegetation, their removal to accommodate the proposed development is considered appropriate. It must be noted that the site is not affected by any environmental overlays and not identified as being of any particular ecological significance. The site has been

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mostly cleared and vacant for many years and does not form part of any landscape corridor or high value environment.

Appropriate provision can be made to 'off-set' the vegetation by way of a permit condition.

6.8 s 173 Agreement

As noted previously, the site is subject to Agreement T148994L which was created 9 June 1994 under permit P.6095G and includes a number of obligations including that:

3. *Dual Occupancy, split blocks under the Victorian Building Code or flats as described in the Pakenham Planning Scheme will not be permitted on any lot with the exception of lot identical with the lot numbered 148 on the endorsed Permit Plan P6095G dated the 24th May 1993.*

A copy of the endorsed plan under Permit P6095G was not able to be found by Council. Given it is not clear that the agreement exempts the subject site from the restriction of dual occupancy, split blocks or flats, a separate request is being made concurrently to end the agreement from the subject site.

The agreement is considered to be 'dead wood' given its age and the strong policy support provided in the Cardinia Planning Scheme and under the Mixed Use Zoning for increased residential density on this site. It would also appear likely that the site is that identified as Lot 148 in the referenced endorsed plans given it was set aside as a super lot for future more intense development.

The removal of the agreement will facilitate the proposed development of the site for townhouses.

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7. Conclusion

Having considered the above assessment, the proposal represents an appropriate development outcome for the site. In particular:

- The proposal is appropriately designed in response to existing conditions on the site and of its surrounding environment.
- The proposal will provide for development intended consistent with the purpose the Mixed Use zoning of the site which includes the promotion of increased housing densities.
- The pattern of subdivision and lot sizes are consistent with the emerging character of surrounding and nearby development.
- The proposal coordinates with and facilitates the orderly development of land in the area.
- The proposed range of lot sizes will contribute to housing diversity
- The proposal is highly compliant with the standards and objectives of Clause 55.

Overall, the proposal is consistent with relevant provisions in the Planning Policy Framework of the Cardinia Planning Scheme.

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Clause 55 ResCode Assessment

CLAUSE AND TITLE	STANDARD	COMPLIES WITH THE STANDARD	ACHIEVES THE OBJECTIVE
55.02-1: Neighbourhood character	B1	Complies	Achieves
55.02-2: Residential policy	B2	Complies	Achieves
55.02-3: Dwelling diversity	B3	Complies	N/A
55.02-4: Infrastructure	B4	Complies	Achieves
55.02-5: Integration with the street	B5	Complies	Achieves
55.03-1: Street setback	B6	Complies	Achieves
55.03-2: Building height	B7	Complies	Achieves
55.03-3: Site coverage	B8	Complies	Achieves
55.03-4: Permeability	B9	Complies	Achieves
55.03-5: Energy efficiency	B10	Complies	Achieves
55.03-6: Open space	B11	Complies	N/A
55.03-7: Safety	B12	Complies	Achieves
55.03-8: Landscaping	B13	Complies	Achieves
55.03-9: Access	B14	Complies	Achieves
55.03-10: Parking location	B15	Complies	Achieves
55.04-1: Side and rear setbacks	B17	Complies	Achieves
55.04-2: Walls on boundaries	B18	Complies	Achieves
55.04-3: Daylight to existing windows	B19	Complies	Achieves
55.04-4: North-facing windows	B20	Complies	Achieves
55.04-5: Overshadowing open space	B21	Complies	Achieves
55.04-6: Overlooking	B22	Complies	Achieves
55.04-7: Internal views	B23	Complies	Achieves
55.04-8: Noise impacts	B24	Complies	Achieves
55.05-1: Accessibility	B25	Complies	Achieves
55.05-2: Dwelling entry	B26	Complies	Achieves
55.05-3: Daylight to new windows	B27	Complies	Achieves
55.05-4: Private open space	B28	Complies	Achieves
55.05-5: Solar access to open space	B29	Variation Required	Achieves
55.05-6: Storage	B30	Complies	Achieves
55.06-1: Design detail	B31	Complies	Achieves
55.06-2: Front fences	B32	Complies	Achieves
55.06-3: Common property	B33	Complies	Achieves

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TITLE AND OBJECTIVE	STANDARD	COMPLIANCE
<p>CLAUSE 55.02-1 NEIGHBOURHOOD CHARACTER</p> <p>To ensure that the design respects the existing neighbourhood character or contributes to a preferred neighbourhood character.</p> <p>To ensure that development responds to the features of the site and the surrounding area.</p>	<p>B1</p> <p>The design response must be appropriate to the neighbourhood and the site.</p> <p>The proposed design must respect the existing or preferred neighbourhood character and respond to the features of the site.</p>	<p>Complies</p> <p>The proposal has been developed from careful analysis of its built form and policy context, with consideration of the existing and preferred neighbourhood character of the area.</p> <p>Refer to the Town Planning Report for a detailed assessment of the proposal against the relevant local policies.</p>
<p>CLAUSE 55.02-2 RESIDENTIAL POLICY</p> <p>To ensure that residential development is provided in accordance with any policy for housing in the State Planning Policy Framework and the Local Planning Policy Framework, including the Municipal Strategic Statement and local planning policies.</p> <p>To support medium densities in areas where development can take advantage of public transport and community infrastructure and services.</p>	<p>B2</p> <p>An application must be accompanied by a written statement to the satisfaction of the responsible authority that describes how the development is consistent with any relevant policy for housing in the State Planning Policy Framework and the Local Planning Policy Framework, including the Municipal Strategic Statement and local planning policies.</p>	<p>Complies</p> <p>The subject site is an appropriate candidate for medium-density infill development while providing an increase in housing diversity within a Mixed Use Zone.</p> <p>Refer to the Town Planning Report for a detailed account and analysis of all statutory and strategic planning policies relevant to the proposal.</p>
<p>CLAUSE 55.02-3 DWELLING DIVERSITY</p> <p>To encourage a range of dwelling sizes and types in developments of ten or more dwellings.</p>	<p>B3</p> <p>Developments of ten or more dwellings should provide a range of dwelling sizes and types, including:</p> <ul style="list-style-type: none"> • Dwellings with a different number of bedrooms. • At least one dwelling that contains a kitchen, bath or shower, and a toilet and wash basin at ground floor level. 	<p>Complies</p> <p>Less than ten dwellings proposed.</p> <p>A range of dwelling types is proposed including reverse living and traditional ground level living spaces.</p>
<p>CLAUSE 55.02-4 INFRASTRUCTURE</p> <p>To ensure development is provided with appropriate utility services and infrastructure.</p>	<p>B4</p> <p>Development should be connected to reticulated services, including reticulated sewerage, drainage, electricity and gas, if available.</p>	<p>Complies</p> <p>The subject site is located in a well-serviced area with existing infrastructure. The proposal has incorporated new internal utility, stormwater mitigation and roads infrastructure to accommodate the</p>

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TITLE AND OBJECTIVE	STANDARD	COMPLIANCE
To ensure development does not unreasonably overload the capacity of utility services and infrastructure.	<p>Development should not unreasonably exceed the capacity of utility services and infrastructure, including reticulated services and roads.</p> <p>In areas where utility services or infrastructure have little or no spare capacity, developments should provide for the upgrading of or mitigation of the impact on services or infrastructure.</p>	residential development on the site to complement existing servicing provisions.
<p>CLAUSE 55.02-5 INTEGRATION WITH THE STREET</p> <p>To integrate the layout of development with the street.</p>	<p>B5</p> <p>Developments should provide adequate vehicle and pedestrian links that maintain or enhance local accessibility.</p> <p>Development should be oriented to front existing and proposed streets.</p> <p>High fencing in front of dwellings should be avoided if practicable.</p> <p>Development next to existing public open space should be laid out to complement the open space.</p>	<p>Complies</p> <p>The development is designed with townhouses at the frontage to Sir Thomas Drive facing the street with the main entry facing the street. This ensure that the development avoids a 'back of house' response with a high degree of articulation and activation to the street provided.</p>

TITLE AND OBJECTIVE	STANDARD	COMPLIANCE
<p>CLAUSE 55.03-1 STREET SETBACK</p> <p>To ensure that the setbacks of buildings from a street respect the existing or preferred neighbourhood character and make efficient use of the site.</p>	<p>B6</p> <p>The same distance as the setback of the front wall of the existing building on the abutting allotment facing the front street or 9 metres, whichever is the lesser.</p>	<p>Complies</p> <p>A minimum 4m setback is required to Sir Thomas Drive given that there are no adjoining properties facing the same street and the site can be considered a corner property.</p> <p>The proposed setback of Townhouses 1 and 16 is 4m to the frontage.</p>
<p>CLAUSE 55.03-2 BUILDING HEIGHT</p> <p>To ensure that the height of buildings respects the existing or preferred neighbourhood character.</p>	<p>B7</p> <p>The maximum building height should not exceed the maximum height specified in the zone, schedule to the zone or an overlay that applies to the land.</p>	<p>Complies</p> <p>The site is within the Mixed Use Zone and there are no height controls under the zone.</p>

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TITLE AND OBJECTIVE	STANDARD	COMPLIANCE
	<p>If no maximum height is specified in the zone, schedule to the zone or an overlay, the maximum building height should not exceed 9 metres.</p> <p>Changes of building height between existing buildings and new buildings should be graduated.</p>	<p>The proposed maximum building height is 7.8m and does not exceed 2 storeys.</p>
<p>CLAUSE 55.03-3 SITE COVERAGE</p> <p>To ensure that the site coverage respects the existing or preferred neighbourhood character and responds to the features of the site.</p>	<p>B8</p> <p>The site area covered by buildings should not exceed 60%.</p>	<p>Complies</p> <p>The proposed overall building site coverage is 60%, meeting the requirement of this standard.</p>
<p>CLAUSE 55.03-4 PERMEABILITY</p> <p>To reduce the impact of stormwater run-off on the drainage system.</p> <p>To facilitate on-site stormwater infiltration.</p>	<p>B9</p> <p>The site area covered by the pervious surfaces should be at least:</p> <ul style="list-style-type: none"> • The minimum area specified in a schedule to the zone; or • If no minimum is specified in a schedule to the zone, 20 percent of the site. 	<p>Complies</p> <p>The proposed permeability of 20% exceeds 20% and therefore meets the requirement of this standard.</p>
<p>CLAUSE 55.03-5 ENERGY EFFICIENCY</p> <p>To achieve and protect energy efficient dwellings and residential buildings.</p> <p>To ensure the orientation and layout of development reduce fossil fuel energy use and make appropriate use of daylight and solar energy.</p>	<p>B10</p> <p>Building should be:</p> <ul style="list-style-type: none"> • Oriented to make appropriate use of solar energy. • Sited and designed to ensure that the energy efficiency of existing dwellings on adjoining lots is not unreasonably reduced. <p>Living areas and private open space should be located on the north side of the development, if practicable.</p> <p>Developments should be designed so that solar access to north-facing windows is maximised.</p>	<p>Complies</p> <p>The development is a logical arrangement of the site having regard to its interfaces with the street, future service station and adjoining properties.</p> <p>The balconies and living areas of each townhouse (other than TH1) are located to the north of the dwelling with excellent access to northern light.</p>
<p>CLAUSE 55.03-6 OPEN SPACE</p> <p>To integrate the layout of development with any public and communal open space provided in or adjacent to the development</p>	<p>B11</p> <p>If any public or communal open space is provided on site, it should:</p> <ul style="list-style-type: none"> • Be substantially fronted by dwellings, where appropriate. • Provide outlook for as many dwellings as practicable. 	<p>Complies</p> <p>Communal space is provided via the shared central driveway and landscape and paths along the north and south boundaries. The dwellings are designed to face onto the paths and communal spaces to ensure a high level of integration and surveillance.</p>

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TITLE AND OBJECTIVE	STANDARD	COMPLIANCE
	<ul style="list-style-type: none"> • Be designed to protect any natural features on the site. • Be accessible and useable. 	
<p>CLAUSE 55.03-7 SAFETY</p> <p>To ensure the layout of development provides for the safety and security of residents and property.</p>	<p>B12</p> <p>Entrances to dwellings and residential buildings should not be obscured or isolated from the street and internal accessways.</p> <p>Planting which creates unsafe spaces along streets and accessways should be avoided.</p> <p>Developments should be designed to provide good lighting, visibility and surveillance of car parks and internal accessways.</p> <p>Private spaces within developments should be protected from inappropriate use as public thoroughfares.</p>	<p>Complies</p> <p>The entrances to the dwellings are clearly defined and easily identifiable from the street and also the communal pedestrian paths.</p>
<p>CLAUSE 55.03-8 LANDSCAPING</p> <p>To encourage development that respects the landscape character of the neighbourhood.</p> <p>To encourage development that maintains and enhances habitat for plants and animals in locations of habitat importance.</p> <p>To provide appropriate landscaping.</p> <p>To encourage the retention of mature vegetation on the site.</p>	<p>B13</p> <p>The landscape layout and design should:</p> <ul style="list-style-type: none"> • Protect any predominant landscape features of the neighbourhood. • Take into account the soil type and drainage patterns of the site. • Allow for intended vegetation growth and structural protection of buildings. • In locations of habitat importance, maintain existing habitat and provide for new habitat for plants and animals. • Provide a safe, attractive and functional environment for residents. 	<p>Complies</p> <p>New landscaping can be accommodated within the site including in the front courtyards of each townhouse, adjacent to the pedestrian and landscape links and along the western boundary.</p>
<p>CLAUSE 55.03-9 ACCESS</p> <p>To ensure the number and design of vehicle crossovers respects the neighbourhood character.</p>	<p>B14</p> <p>The width of accessways or car spaces should not exceed:</p> <ul style="list-style-type: none"> • 33 per cent of the street frontage, or • if the width of the street frontage is less than 20 metres, 40 per cent of the street frontage. <p>No more than one single-width crossover should be provided for each</p> 	<p>Complies</p> <p>The proposed crossover 14% of the site and comply with the maximum 33%.</p>

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TITLE AND OBJECTIVE	STANDARD	COMPLIANCE
	<p>dwelling fronting a street. The location of crossovers should maximise the retention of on-street car parking spaces. The number of access points to a road in a Road Zone should be minimised.</p> <p>Developments must provide for access for service, emergency and delivery vehicles.</p>	
<p>CLAUSE 55.03-10 PARKING LOCATION</p> <p>To provide convenient parking for resident and visitor vehicles.</p> <p>To protect residents from vehicular noise within developments.</p>	<p>B15</p> <p>Car parking facilities should:</p> <ul style="list-style-type: none"> • Be reasonably close and convenient to dwellings and residential buildings. • Be secure. • Be well ventilated if enclosed. <p>Shared accessways or car parks of other dwellings and residential buildings should be located at least 1.5 metres from the windows of habitable rooms. This setback may be reduced to 1 metre where there is a fence at least 1.5 metres high or where window sills are at least 1.4 metres above the accessway.</p>	<p>Complies</p> <p>Car parking is securely provided in double garages for each dwelling with internal dimensions which comply with Clause 52.06.</p>

TITLE AND OBJECTIVE	STANDARD	COMPLIANCE
<p>CLAUSE 55.04-1 SIDE AND REAR SETBACKS</p> <p>To ensure that the height and setback of a building from a boundary respects the existing or preferred neighbourhood character and limits the impact on the amenity of existing dwellings.</p>	<p>B17</p> <p>A new building not on or within 200mm of a boundary should be set back from side or rear boundaries:</p> <p><i>A new building not on or within 200mm of a boundary should be set back 1 metres from the side boundary, and 3 metres from the rear boundary, plus 0.3 metres for every metre of height over 3.6 metres up to 6.9 metres, plus 1 metres for every metre of height over 6.9 metres.</i></p>	<p>Complies</p> <p>All side and rear setbacks comply with the minimum side and rear setbacks of this standard.</p> <p>Elevations are provided with a high degree of articulation to reduce the extent of visual bulk.</p>

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TITLE AND OBJECTIVE	STANDARD	COMPLIANCE
	<p>Sunblinds, verandahs, porches, eaves, fascias, gutters, masonry chimneys, flues, pipes, domestic fuel or water tanks, and heating or cooling equipment or other services may encroach not more than 0.5 metres into the setbacks of this standard.</p> <p>Landings having an area of not more than 2 square metres and less than 1 metre high, stairways, ramps, pergolas, shade sails and carports may encroach into the setbacks of this standard.</p>	
<p>CLAUSE 55.04-2 WALLS ON BOUNDARIES</p> <p>To ensure that the location, length and height of a wall on a boundary respects the existing or preferred neighbourhood character and limits the impact on the amenity of existing dwellings.</p>	<p>B18</p> <p>A new wall that is on or within 200mm of a side or rear boundary of a lot or a carport constructed on or within 1m of a side or rear boundary of a lot, should not abut the boundary</p> <ul style="list-style-type: none"> • For a length more than the distance specified in a schedule to the zone; or • If no distance is specified in a schedule to the zone, for a length of more than: <ul style="list-style-type: none"> ○ 10m plus 25 % of the remaining length of the boundary of an adjoining lot, or ○ Where there are existing or simultaneously constructed wall/carport abutting the boundary on an abutting lot, the length of the existing or simultaneously constructed walls or carports, <p>Whichever is the greater.</p> <p>A new wall or carport may fully abut a side or rear boundary where slope and retaining walls or fences would result in the effective height of the wall or carport being less than 2 metres on the abutting property boundary.</p> <p>A building on a boundary includes a building set back up to 200mm from a boundary.</p> <p>The height of a new wall constructed on or within 200mm of a side or rear boundary or a carport constructed on or within 1 metre of a side or rear boundary should not exceed an average of 3.2 metres with no part higher than 3.6 metres unless abutting</p>	<p>Complies</p> <p>There are no new walls proposed to be built on the boundaries of the site.</p>

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TITLE AND OBJECTIVE	STANDARD	COMPLIANCE
	a higher existing or simultaneously constructed wall.	
<p>CLAUSE 55.04-3 DAYLIGHT TO EXISTING WINDOWS</p> <p>To allow adequate daylight into existing habitable room windows.</p>	<p>B19</p> <p>Buildings opposite an existing habitable room window should provide for a light court to the existing window that has a minimum area of 3 square metres and minimum dimension of 1 metre clear to the sky.</p> <p>The calculation of the area may include land on the abutting lot.</p> <p>Walls or carports more than 3 metres in height opposite an existing habitable room window should be set back from the window at least 50 per cent of the height of the new wall if the wall is within a 55 degree arc from the centre of the existing window. The arc may be swung to within 35 degrees of the plane of the wall containing the existing window.</p> <p>Where the existing window is above ground floor level, the wall height is measured from the floor level of the room containing the window.</p>	<p>Complies</p> <p>All buildings proposed opposite existing habitable room windows are setback in accordance with this standard ensuring that there is adequate daylight maintained to adjoining properties.</p>
<p>CLAUSE 55.04-4 NORTH-FACING WINDOWS</p> <p>To allow adequate solar access to existing north-facing habitable room windows.</p>	<p>B20</p> <p>If a north-facing habitable room window of an existing dwelling is within 3 metres of a boundary on an abutting lot, a building should be setback from the boundary 1 metre, plus 0.6 metres for every metre of height over 3.6 metres up to 6.9 metres, plus 1 metre for every metre of height over 6.9 metres, for a distance of 3 metres from the edge of each side of the window. A north-facing window is a window with an axis perpendicular to its surface oriented north 20 degrees west to north 30 degrees east.</p>	<p>N/A</p> <p>There are no north facing windows of adjoining properties within 3m of the subject.</p>
<p>CLAUSE 55.04-5 OVERSHADOWING OPEN SPACE</p> <p>To ensure buildings do not significantly overshadow existing secluded private open space.</p>	<p>B21</p> <p>Where sunlight to the secluded private open space of an existing dwelling is reduced, at least 75 per cent, or 40 square metres with minimum dimension of 3 metres, whichever is the lesser area, of the secluded private open space should receive a minimum of five hours of sunlight between 9 am and 3 pm on 22 September.</p>	<p>Complies</p> <p>The proposed development does not overshadow for 5 hours between 9am and 3pm on equinox for adjoining properties. Due to the orientation of the lot overshadowing is mostly limited to within the subject site and does not extend beyond the shadows cast by existing boundary fencing.</p>

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TITLE AND OBJECTIVE	STANDARD	COMPLIANCE
	If existing sunlight to the secluded private open space of an existing dwelling is less than the requirements of this standard, the amount of sunlight should not be further reduced.	
CLAUSE 55.04-6 OVERLOOKING To limit views into existing secluded private open space and habitable room windows.	B22 A habitable room window, balcony, terrace, deck or patio should be located and designed to avoid direct views into the secluded private open space of an existing dwelling within a horizontal distance of 9 metres (measured at ground level) of the window, balcony, terrace, deck or patio. Views should be measured within a 45 degree angle from the plane of the window or perimeter of the balcony, terrace, deck or patio, and from a height of 1.7 metres above floor level.	Complies Where windows associated with the dwellings are located within the 9m arc to the open space of adjoining properties, appropriate screening measures have been incorporated into the design including highlight windows or fixed obscure glazing to 1.7m above finished floor levels.
CLAUSE 55.04-7 INTERNAL VIEWS To limit views into the secluded private open space and habitable room windows of dwellings and residential buildings within a development.	B23 Windows and balconies should be designed to prevent overlooking of more than 50 percent of the secluded private open space of a lower-level dwelling or residential building directly below and within the same development.	Complies Windows are positioned to prevent overlooking between dwellings.
CLAUSE 55.04-8 NOISE IMPACTS To contain noise sources in developments that may affect existing dwellings. To protect residents from external noise.	B24 Noise sources, such as mechanical plant, should not be located near bedrooms of immediately adjacent existing dwellings. Noise sensitive rooms and secluded private open spaces of new dwellings and residential buildings should take account of noise sources on immediately adjacent properties. Dwellings and residential buildings close to busy roads, railway lines or industry should be designed to limit noise levels in habitable rooms.	Complies The proposed development has no unreasonable noise sources. In respect to noise from the approved future service station on the northern portion of the site, the townhouses will be separated by a future 16m wide council standard road which will provide noise attenuation to the townhouses. Additional attenuation can be achieved with the use of double glazing to the north facing windows of townhouses 17-26.

TITLE AND OBJECTIVE	STANDARD	COMPLIANCE
CLAUSE 55.05-1 ACCESSIBILITY To encourage the consideration of the needs of people with	B25 The dwelling entries of the ground floor of dwellings and residential buildings should be accessible or able to be	Complies The ground-floor entries to the dwellings are relative to natural ground level.

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TITLE AND OBJECTIVE	STANDARD	COMPLIANCE
limited mobility in the design of developments.	easily made accessible to people with limited mobility.	
CLAUSE 55.05-2 DWELLING ENTRY OBJECTIVE To provide each dwelling or residential building with its own sense of identity.	B26 Entries to dwellings and residential buildings should: <ul style="list-style-type: none"> • Be visible and easily identifiable from streets and other public areas. • Provide shelter, a sense of personal address and a transitional space around the entry. 	Complies Each dwelling has an indented entry and as such creates a portico like entry area that is accessible via an identifiable path and as such gives each dwelling its own sense of identity and a sense of address.
CLAUSE 55.05-3 DAYLIGHT TO NEW WINDOWS To allow adequate daylight into new habitable room windows	B27 A window in a habitable room should be located to face: <ul style="list-style-type: none"> • An outdoor space clear to the sky or a light court with a minimum area of 3 square metres and minimum dimension of 1 metre clear to the sky, not including land on an abutting lot, or • A verandah provided it is open for at least one third of its perimeter, or • A carport provided it has two or more open sides and is open for at least one third of its perimeter. 	Complies The proposed development will allow adequate daylight into all habitable room windows. All windows face an outdoor space with a minimum area of 3 square metres and minimum dimension of 1 metre clear to the sky.
CLAUSE 55.05-4 PRIVATE OPEN SPACE To provide adequate private open space for the reasonable recreation and service needs of residents	B28 A dwelling or residential building should have private open space of an area and dimensions specified in the schedule to the zone. If no area or dimensions are specified in the schedule to the zone, a dwelling or residential building should have private open space consisting of: <ul style="list-style-type: none"> • An area of 40 square metres, with one part of the private open space to consist of secluded private open space at the side or rear of the dwelling or residential building with a minimum area of 25 square metres, a minimum dimension of 3 metres and convenient access from a living room, or • A balcony of 8 square metres with a minimum width of 1.6 metres and convenient access from a living room, or • A roof-top area of 10 square metres with a minimum width of 2 metres 	Complies Townhouses 1 and 26 are provided a ground level SPOS which has an areas of 25sqm with an additional first floor balcony of 6sqm. The remaining townhouses are designed in a reverse living arrangement with balconies ranging is size from 12sqm to 14sqm with direct access from an internal living area. The balconies are at the narrowest 2m wide.

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TITLE AND OBJECTIVE	STANDARD	COMPLIANCE
	and convenient access from a living room.	
CLAUSE 55.05-5 SOLAR ACCESS TO OPEN SPACE To allow solar access into the secluded private open space of new dwellings and residential buildings.	B29 The private open space should be located on the north side of the dwelling or residential building, if appropriate. The southern boundary of secluded private open space should be set back from any wall on the north of the space at least $(2 + 0.9h)$ metres, where 'h' is the height of the wall.	Variation Required A minor variation is required for the SPOS of TH1 which is located on the south side of the dwelling at ground level. While the area is to the south, it will received an adequate amount of solar access as it is open to the east and will received good levels of sunlight from the morning period through to midday. On balance, given that only 1 of the townhouses does not meet this standard and all other townhouses have direct northern SPOS areas it is considered appropriate to allow a variation in this instance.
CLAUSE 55.05-6 STORAGE To provide adequate storage facilities for each dwelling.	B30 Each dwelling should have convenient access to at least 6 cubic metres of externally accessible, secure storage space.	Complies Secure externally accessible storage of 6 cubic metres is provided for each dwelling within the garage space at basement level.

TITLE AND OBJECTIVE	STANDARD	COMPLIANCE
CLAUSE 55.06-1 DESIGN DETAIL To encourage design detail that respects the existing or preferred neighbourhood character.	B31 The design of buildings, including: <ul style="list-style-type: none"> • Façade articulation and detailing; • Windows and door proportions; • Roof form; and • Verandahs, eaves and parapets. Should respect the existing or preferred neighbourhood character. Garages and carports should be visually compatible with development and the existing or preferred neighbourhood character.	Complies The contemporary architectural appearance of the development draws reference from the positive design elements of surrounding dwellings, incorporating design details of both original housing stock and newer development. The provision of a flat roof will ensure the overall height of the buildings are kept low while noting that the site is within a MUZ and designed for higher density development.
CLAUSE 55.06-2 FRONT FENCES To encourage front fence design that	B32 The design of front fences should complement the design of the dwelling or	Complies Low semi-transparent front fencing is proposed along Sir Thomas Drive.

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TITLE AND OBJECTIVE	STANDARD	COMPLIANCE
<p>respects the existing or preferred neighbourhood character.</p>	<p>residential building and any front fences on adjoining properties.</p> <p>A front fence within 3 metres of a street should not exceed:</p> <ul style="list-style-type: none"> • The maximum height specified in the schedule to the zone, or • If no maximum height is specified in the schedule to the zone, the maximum height specified in Table B3. That is, 2m for Streets in a Road Zone, Category 1 and 1.5 metres for other streets. 	
<p>CLAUSE 55.06-3 COMMON PROPERTY</p> <p>To ensure that communal open space, car parking, access areas and site facilities are practical, attractive and easily maintained.</p> <p>To avoid future management difficulties in areas of common ownership.</p>	<p>B33</p> <p>Developments should clearly delineate public, communal and private areas.</p> <p>Common property, where provided, should be functional and capable of efficient management.</p>	<p>Complies</p> <p>Common property is clearly delineated and will managed by the owners corporation for maintenance.</p>
<p>CLAUSE 55.06-4 SITE SERVICES</p> <p>To ensure that site services can be installed and easily maintained.</p> <p>To ensure that site facilities are accessible, adequate and attractive.</p>	<p>B34</p> <p>The design and layout of dwellings and residential buildings should provide sufficient space (including easements where required) and facilities for services to be installed and maintained efficiently and economically.</p> <p>Bin and recycling enclosures, mailboxes and other site facilities should be adequate in size, durable, waterproof and blend in with the development.</p> <p>Bin and recycling enclosures should be located for convenient access by residents.</p> <p>Mailboxes should be provided and located for convenient access as required by Australia Post.</p>	<p>Complies</p> <p>The proposal incorporates sufficient provision for services, bin storage and mailboxes and can be conveniently accessed by all residents.</p>

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Connect Town Planning
PO BOX 402 Sandringham Vic 3191
0400 829 235
phillipr@connectplanning.com.au
www.connectplanning.com.au



ARBORICULTURAL REPORT

LOT H, PRINCES HIGHWAY, PAKENHAM

April 2022

PREPARED BY



Consultant Arborist
DipArb

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LANDSCAPE ARCHITECTS
ENVIRONMENTAL HORTICULTURISTS
LANDSCAPE HERITAGE CONSULTANTS
CONSULTANT ARBORISTS

JOHN PATRICK LANDSCAPE ARCHITECTS PTY LTD

324 Victoria Street, Richmond, VIC 3121, Australia
T +61 3 9429 4855 E admin@johnpatrick.com.au
F +61 3 9429 8211 W www.johnpatrick.com.au

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1 Introduction

- 1.1 [REDACTED] consulting arborists, have been engaged by J T Pakenham Pty Ltd to prepare an arboricultural report for Lot H, Princes Highway, Pakenham to accompany planning application documents for the site.

2 Objectives

- 2.1 The intent of this report is to:

- Assess the condition of trees within the subject site and those neighbouring that may be impacted by the proposed development and estimate the extent of any impact.
- Identify any trees worthy of retention and provide preliminary arboricultural advice to assist in their protection and retention.

- 2.2 The report will include the following;

- Botanic / Common names
- Tree Location
- Canopy width and height
- DBH (trunk diameter)
- Tree health & structure condition
- Useful Life Expectancy (ULE)
- Tree Protection Zones (TPZ's) in accordance with AS4970
- Arboricultural value
- Other tree characteristics of consideration.

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3 Methodology

- 3.1 The site was visited on the 04th of April 2022 and a visual assessment of the subject trees was undertaken from ground level. Each tree was assigned an identification number for reference purposes, denoted on the

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attached Tree Location Plan (Section 4) which is based on aerial imaging and the Title Re-establishment, Feature, and Level Survey prepared for the site by Land Surveys, Job No. 2205564 – RFL – 001, 28/03/22.

- 3.2 No aerial or diagnostic testing was undertaken as part of this assessment.
- 3.3 The DBH of trees was measured using a diameter tape measure at 1.4m above ground level in accordance with AS4970.
- 3.4 Heights of canopies were measured using a laser range finder.
- 3.5 Widths of canopies were measured by stepping out.
- 3.6 Where access directly to the trees was not possible DBH, heights and widths were estimated.

4 Observations

EXISTING CONDITIONS

The subject site is located on the south-west corner of Princes Highway and Sir Thomas Drive. Currently it exists as an overgrown site with areas of gorse and blackberries. Other vegetation includes a number of acacia and eucalyptus species as well as several weedy taxa.



Fig. 1: Subject Site (Nearmap.com 04/02/2022)

VEGETATION CONTROLS

- 4.1 An internet search of 'VicPlan' reveals that the site is covered by no overlays pertaining to tree control of the Cardinia Council Planning Scheme.
- 4.2 Cardinia Local Law 17 applies to the site. Under this law there are no tree control requirements.
- 4.3 As the site is greater than 4000m², *Clause 52.17 - Native Vegetation* applies:

52.17 Native Vegetation

Purpose

To ensure that there is no net loss to biodiversity as a result of the removal, destruction or lopping of native vegetation. This is achieved by applying the following three step approach in accordance with the Guidelines for the removal, destruction or lopping of native vegetation (Department of Environment, Land, Water and Planning, 2017) (the Guidelines):

- 1. Avoid the removal, destruction or lopping of native vegetation.*
- 2. Minimise impacts from the removal, destruction or lopping of native vegetation that cannot be avoided.*
- 3. Provide an offset to compensate for the biodiversity impact if a permit is granted to remove, destroy, or lop native vegetation.*
- 4. To manage the removal, destruction or lopping of native vegetation to minimise land and water degradation.*

- 4.4 Several trees on site are native to Victoria and will require an offset for their removal. *Clause 52.17-7 Table of exemptions -Regrowth* exempts native vegetation that is less than ten years old on land that has previously been cleared. Using historic aerial images trees that are less than 10 years old have been identified (Appendix 1 and 2).
See section 5. *Discussion* for more information on which trees are exempt.
- 4.5 The services of an ecologist will be required to fully assess the origin of vegetation other than trees such as grasses, plants, shrubs, etc which may require permits and offsets for their removal.

****Note: It is recommended that vegetation controls be confirmed with the local authority prior to any tree removal.***

TREE INFORMATION

- 4.6 A total of 30 trees, tree groups, or areas were assessed including 16 trees, tree groups, or areas within the subject site and 14 trees within the road reserve or neighbouring properties. Information on these can be found in the following table.

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TREE DATA

Table 1: Tree Data												
Tree No.	Botanic Name	Common Name	Origin	Size (m) HXW	DBH (cm)	TPZ (m)	Age	Health	Structure	ULE (Yrs.)	Arb Value	Comments
1	<i>Acacia longifolia</i> subsp. <i>longifolia</i>	Sallow Wattle	Vic. Native	6.5 x 5.5	22	2.6	Mature	Good	Fair	5-10	low	Failed stem, multi-stemmed form
2	<i>Acacia longifolia</i> subsp. <i>sophorae</i>	Sallow Wattle	Vic. Native	6 x 4	17	2.0	Mature	Good	Good	5-10	low	
3	<i>Acacia longifolia</i> subsp. <i>sophorae</i>	Sallow Wattle	Vic. Native	5.5 x 3.5	11	2.0	Mature	Fair	Fair	5-10	Low	
4	<i>Acacia longifolia</i> subsp. <i>sophorae</i>	Sallow Wattle	Vic. Native	5.5 x 4	15	2.0	Mature	Good	Fair	5-10	Low	
5	<i>Corymbia maculata</i>	Spotted Gum	Aust. Native	13 x 5.5	38	4.6	Mature	Good	Poor	5-10	Low	Multi-stemmed form
6	<i>Eucalyptus</i> sp.	Gum	Aust. Native	7 x 4	23	2.8	Mature	Poor	Poor	0-5	Low	Bifurcated, failed stem, tip dieback
7	<i>Acacia longifolia</i> subsp. <i>longifolia</i>	Sallow Wattle	Vic. Native	7 x 7	23	2.8	Mature	Fair	Fair	5-10	Low	
8	<i>Acacia mearnsii</i>	Late Black Wattle	Vic. Native	6 x 3.5	15	2.0	Mature	Good	Good	5-10	Low	
9	<i>Eucalyptus melliodora</i>	Yellow Box	Vic. Native	6.5 x 10	39	4.7	Mature	Good	Poor	5-10	Low	Large trunk wound
10	<i>Corymbia maculata</i>	Spotted Gum	Aust. Native	8 x 3	13	2.0	Mature	Good	Good	10-20	Low	
11	<i>Acacia mearnsii</i>	Late Black Wattle	Vic. Native	7 x 8.5	21	2.5	Mature	Good	Good	5-10	Low	
12	<i>Acacia mearnsii</i>	Late Black Wattle	Vic. Native	6.5 x 6	15	2.0	Mature	Good	Fair	5-10	Low	
13	<i>Prunus</i> sp.	Stone Fruit	Exotic	2.5 x 2.5	<17	2.0	Semi-mature	Poor	Unknown	5-10	Low	Unable to see form or base, growing next to fence
14	<i>Malus</i> sp.	Apple	Exotic	4.5 x 3	16	2.0	Mature	Good	Fair	10-20	Low	~0.5m from fence line, lopped
15	<i>Citrus limon</i>	Lemon	Exotic	4.5 x 4.5	Multi	2.0	Mature	Good	Fair	10-20	Low	
16	<i>Cupressus sempervirens</i>	Italian Cypress	Exotic	3.5 x 1*	10	2.0	Mature	Good	Good	10-20	Low	x3 trees. *=average
17	<i>Prunus persica</i> var. <i>universitas</i>	Nectarine	Exotic	3 x 4.5	<17	2.0	Mature	Fair	Fair	5-10	Low	
18	<i>Citrus limon</i>	Lemon	Exotic	3 x 3	<17	2.0	Mature	Fair	Fair	10-20	Low	
19	<i>Acacia mearnsii</i>	Late Black Wattle	Vic. Native	6 x 5	16	2.0	Mature	Good	Good	5-10	Low	
20	<i>Eucalyptus</i> sp.	Gum	Aust. Native	7 x 7	20	2.4	Mature	Good	Fair	5-10	Low	
21	<i>Eucalyptus leucoxylon</i>	Yellow Gum	Vic. Native	7 x 8	20	2.4	Mature	Good	Good	10-20	Medium	
22	<i>Eucalyptus</i> sp.	Gum	Aust. Native	12 x 9	26	3.1	Mature	Good	Good	10-20	Medium	Minor deadwood
23	<i>Viburnum</i> sp.	Viburnum	Exotic	4 x 4	11	2.0	Mature	Fair	Poor	5-10	Low	
24	<i>Corymbia citriodora</i>	Lemon-scented Gum	Aust. Native	12.5 x 8	27	3.2	Mature	Fair	Fair	5-10	Low	Bifurcated at ~2.0m, thin canopy, advanced sucker growth
25	<i>Melaleuca ericifolia</i>	Swamp Paperbark	Aust. Native	4 x 3*	<17*	2.0	Mature	Fair	Fair-Poor	10-20	Low	Multiple trees, *=average
26	<i>Acacia implexa</i>	Lightwood	Vic. Native	10 x 9#	26*	3.1	Mature	Good	Fair	5-10	Medium	x2 trees, #=shared canopy, *=average
A	Gully overgrown with blackberries. <i>Acacia</i> sp. and <i>Eucalyptus</i> sp. saplings present											
B	Corymbia maculata regrowth, less than 10cm DBH											
C	Inaccessible area overgrown with blackberries and gorse. Trees within area: <i>Acacia mearnsii</i> , <i>Acacia implexa</i> , <i>Eucalyptus</i> sp., <i>Fraxinus</i> sp., and other weedy taxa. Largest DBH estimated at ~17cm											
D	Row of semi-mature <i>Acacia</i> sp. and other shrubs											

NOTE: Values marked in red represent estimates
Values marked in blue represent multi-stem calculations

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TREE IMAGES



Fig. 2: Tree 1.



Fig. 3: Tree 2.



Fig. 4: Trees 3.



Fig. 5: Tree 5.



Fig. 6: Tree 6.



Fig. 7: Tree 7.



Fig. 8: Trees 8.



Fig. 9: Tree 9.

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Fig. 10: Tree 10.



Fig. 11: Tree 11.



Fig. 12: Tree 12.



Fig. 13: Neighbouring Tree 13



Fig. 14: Neighbouring Tree 14.



Fig. 15: Neighbouring Tree 15.



Fig. 16: Neighbouring Trees 16, 17, and 18(part).

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Fig. 17: Tree 19.



Fig. 18: Neighbouring Trees 20-22.

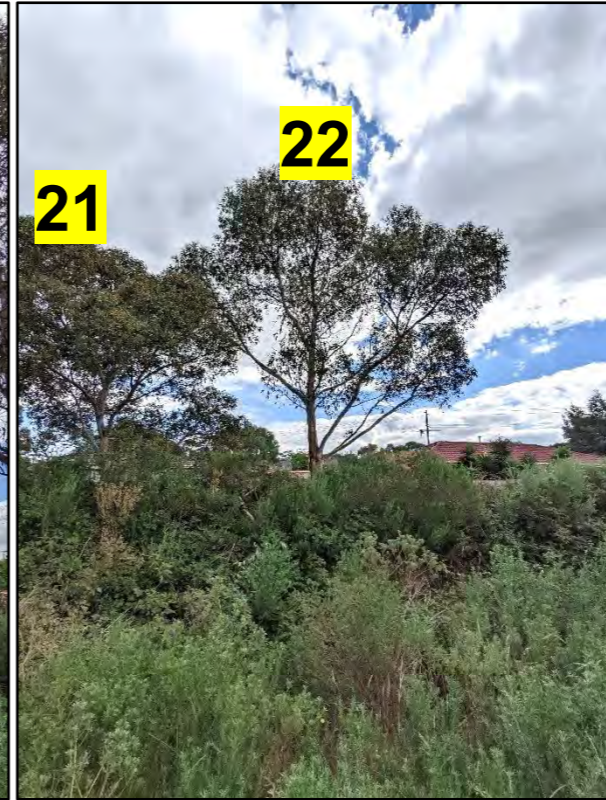


Fig. 19: Neighbouring Trees 21 and 22.



Fig. 20: Neighbouring Tree 23



Fig. 21: Street Tree 24



Fig. 22: Tree Group 25.



Fig. 23: Tree Group 26.

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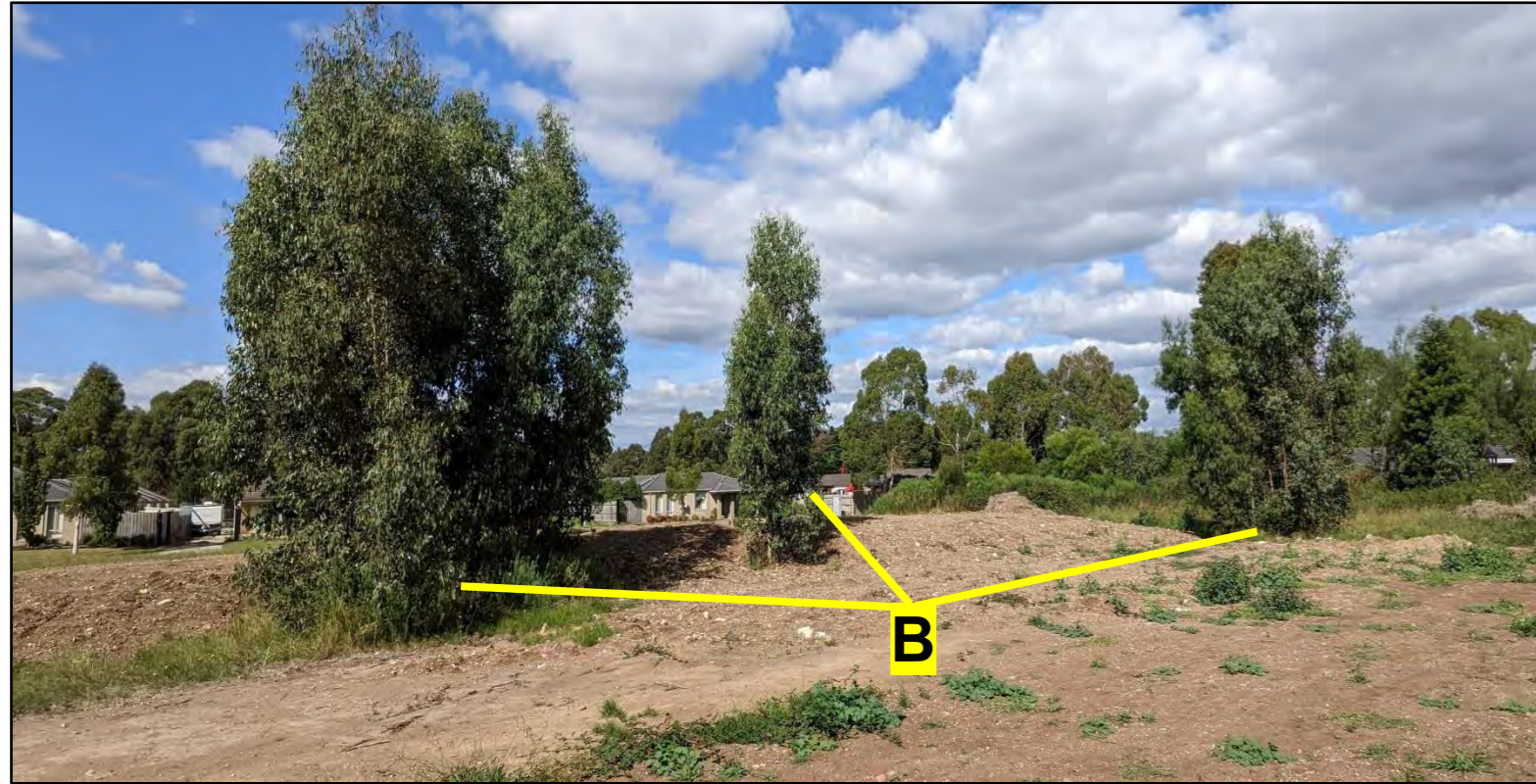


Fig. 24: Tree Group B.

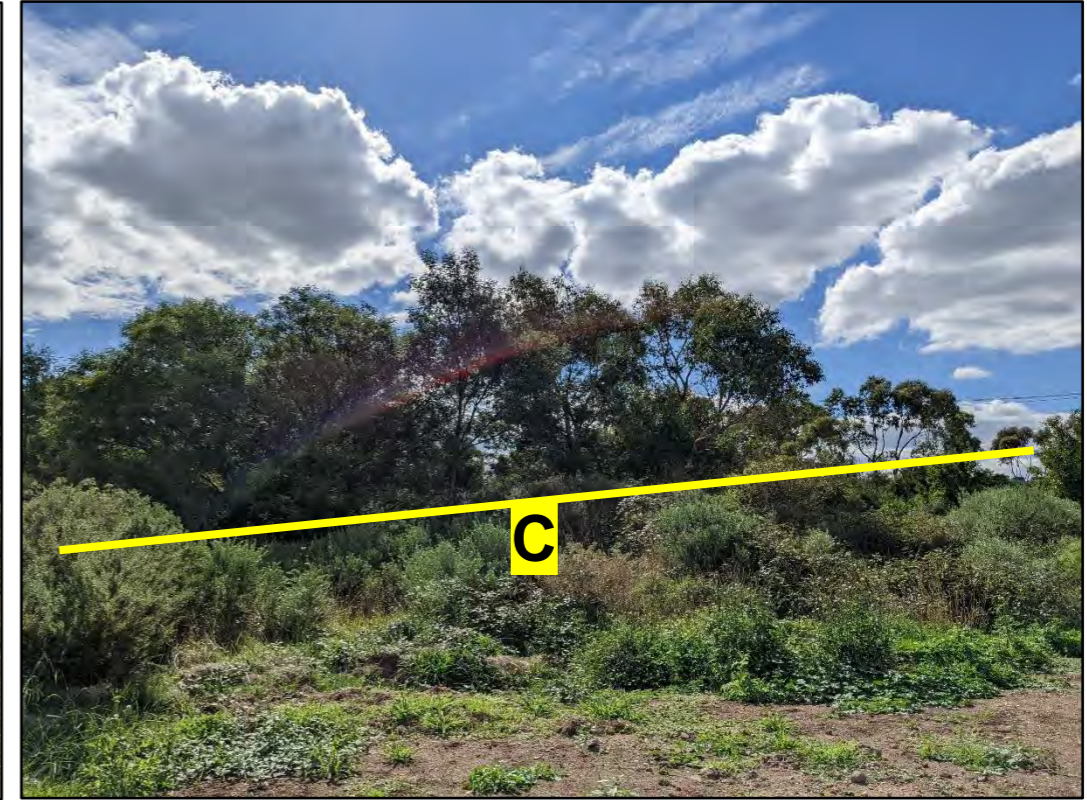


Fig. 25: Area C.

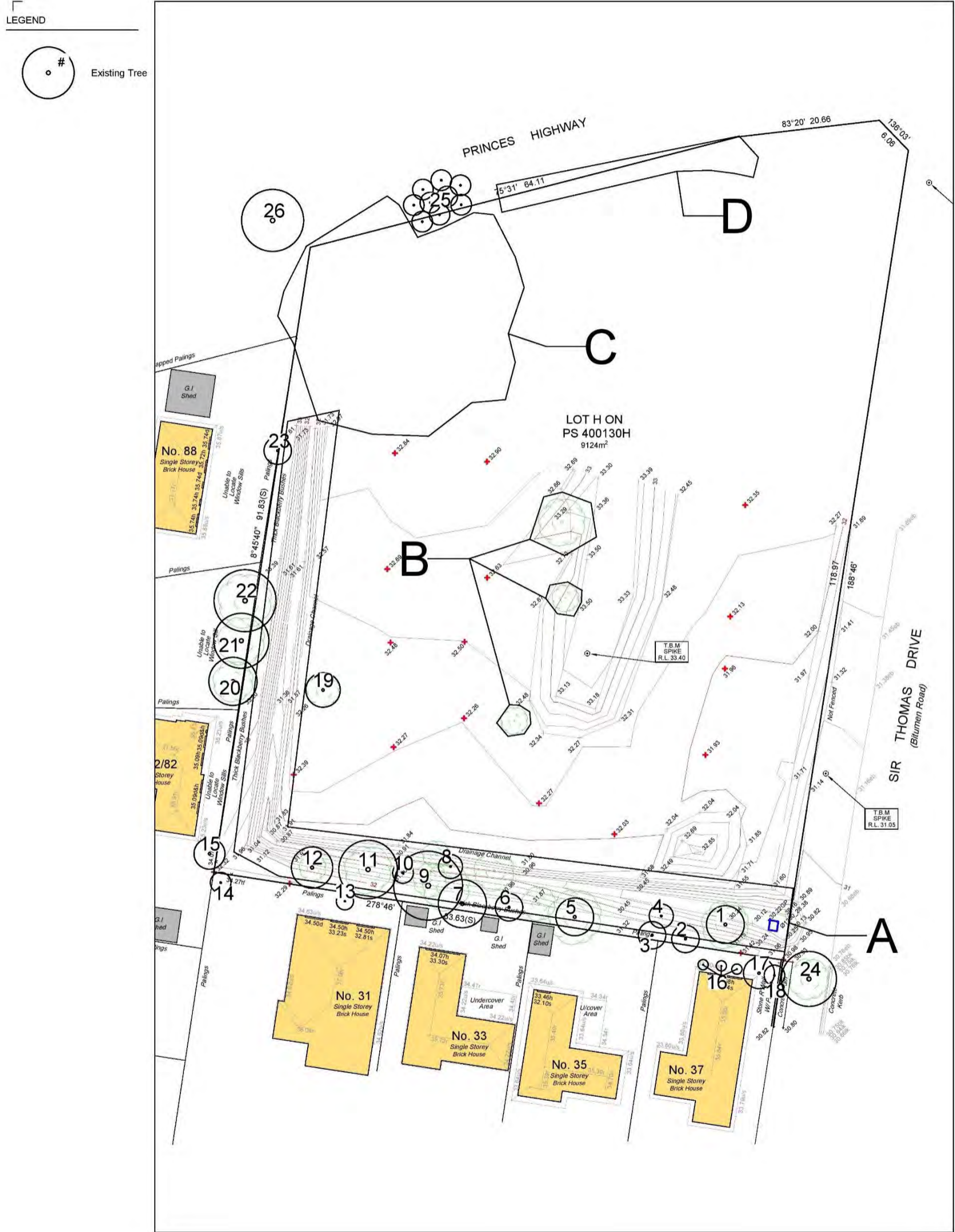


Fig. 26: Area D.

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TREE LOCATION PLAN



JOHN PATRICK
LANDSCAPE ARCHITECTS PTY LTD
324 Victoria Street,
Richmond, VIC 3121
T +61 3 9429 4855
F +61 3 9429 8211
admin@johnpatrick.com.au
www.johnpatrick.com.au

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PROJECT
Residential and Commercial Development

ADDRESS
Lot H, Princes Highway, Pakenham

DRAWING
Tree Location Plan



SCALE 1:500 @A3
DATE 20-04-2022
DRAWN JH
CHECKED
JOB NO 22-146
DWG NO TLP01

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5 Discussion

- 5.1 A block of townhouses and a service station with associated convenience shops are proposed for the site. The following plans have been reviewed and form the basis of the following impact assessment:

Proposed Residences, Princes Highway, Pakenham

Ground Floor – TP01

Prepared by Valeo Constructions

- 5.2 This report assumes that the levels, dimensions, and drawings provided by the surveyors and architects named within this report are correct as these have been used as the basis for this impact assessment.
- 5.3 An aerial survey of the site was used to locate trees within the site as a feature survey was not available at the time. Their location on the Tree Location Plan and Impact Assessment Plan are therefore based on a visual estimation. Any comments regarding the impact on these trees are approximate only.

SITE TREES

- 5.4 All site trees are proposed to be removed to accommodate the development.
- 5.5 Area A is a drainage channel running along the southern and western boundaries of the site. The area is overgrown with blackberries and gorse weed making it difficult to access in parts. Acacia and eucalyptus saplings were noted throughout the area as well as other exotic weedy species.
- 5.6 Trees 1-4 and 7 are *Acacia longifolia* subsp. *longifolia* (Sallow Wattle) or *Acacia longifolia* subsp. *sophorae* (Sallow Wattle) located along the southern boundary within Area A. They are of good to fair health and structure, of low arboricultural value, and not worthy of retention. These species are native to Victoria and are covered by *Clause 52.17*. Trees 1, 3, and 7 are estimated to be less than ten years old and are exempt from the Clause as per *52.17-7 Table of exemptions -Regrowth* (Appendix 1). Trees 2 and 4 are estimated to be more than ten years old and permits will be required for their removal or major works (lopping).
- 5.7 Trees 5 and 10 are *Corymbia maculata* (Spotted Gum) located along the southern boundary within Area A. Both are of good health while Tree 5 is of poor structure with a multi-stemmed form. It is of low arboricultural value and not worthy of retention. Tree 10 is of good structure but is an unremarkable specimen. It is also of low arboricultural value and not worthy of retention.
- 5.8 Tree 6 is a *Eucalyptus* sp. (Gum) located along the southern boundary within Area A. It is of poor health and structure with failed stems and canopy dieback. It is of low arboricultural value and not worthy of retention. Due to the excessive growth of blackberries it was difficult to gain access to the tree in order to collect samples for a positive species identification. It is unknown if this tree is native to Victoria and covered by

Clause 52.17. Regardless, it is estimated this tree is less than 10 years old and is exempt from the Clause as per *52.17-7 Table of exemptions -Regrowth* (Appendix 1).

- 5.9 Trees 8, 11, and 12 are all *Acacia mearnsii* (Late Black Wattle) located along the southern boundary within Area A. These trees are of good health and good to fair structure. They are unremarkable specimens, of low arboricultural value, and not worthy of retention. This species is native to Victoria and are estimated to be less than ten years old. They are exempt from *Clause 52.17* as per *52.17-7 Table of exemptions -Regrowth* (Appendix 1).
- 5.10 Tree 9 is a *Eucalyptus melliodora* (Yellow Box) located along the southern boundary within Area A. It is of good health but poor structure with a large trunk wound compromising its integrity. It is of low arboricultural value and not worthy of retention. This species is native to Victoria, is estimated to be less than 10 years old, and is exempt from *Clause 52.17* as per *52.17-7 Table of exemptions -Regrowth* (Appendix 1).
- 5.11 Tree 19 is an *Acacia mearnsii* (Late Black Wattle) located towards the western boundary of the site. It is of good health and structure. It is an unremarkable specimen, of low arboricultural value, and not worthy of retention. This species is native to Victoria, is estimated to be less than 10 years old, and is exempt from *Clause 52.17* as per *52.17-7 Table of exemptions -Regrowth* (Appendix 1).
- 5.12 Tree Group B are groups of *Corymbia maculata* (Spotted Gum) regrowth located down the centre of the site. Their DBH's are at or below 10cm, they are of low arboricultural value, and they are not worthy of retention.
- 5.13 Area C is an overgrown section of the site located in the north-west corner. It was inaccessible due to the dense growth of blackberries and gorse. Trees within the area could not be accessed to be measured. Species noted in the area were *Acacia mearnsii* (Late Black Wattle), *Acacia implexa* (Lightwood), *Eucalyptus sp.* (Gum), *Fraxinus sp.* (Ash), and other weedy taxa. The largest tree DBH in the area is estimated to be ~17cm. Some of these species are native to Victoria and may be over 10 years old. These trees may be subject to *Clause 52.17* and require a permit for their removal. A more detailed study of the area will need to be conducted once access to the trees can be obtained.
- 5.14 Area D is a row of semi-mature *Acacia sp.* (Wattle) and other shrubs located along the northern boundary of the site. All trees have DBH's below 10cm, are of low arboricultural value, and are not worthy of retention. Most of this area has been cleared in the past (Appendix 1 and 2) though it is difficult to establish what vegetation has been left untouched. Based on the size of the existing trees it is estimated no vegetation is greater than 10 years old and it is therefore exempt from *Clause 52.17* as per *52.17-7 Table of exemptions -Regrowth*.

NEIGHBOURING TREES

- 5.15 Trees 13-18 are a variety of exotic tree within the neighbouring yards along the southern boundary of the site. All have TPZs of 2.0m which are not encroached by the proposed development. If the ground within their TPZs are kept at existing levels, they are not expected to be impacted.

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- 5.16 Tree 20 is a *Eucalyptus sp.* (Gum) located in the neighbouring yard to the west of the subject site. Its TPZ of 2.4m is encroached by 12.0% caused by the proposed driveway and visitor car area. This is just above the 10% considered a minor encroachment under *AS4970 Protection of trees on development sites*. The development is not expected to impact the long-term health of the tree.
- 5.17 Trees 21 and 22 are a *Eucalyptus leucoxylon* (Yellow Gum) and *Eucalyptus sp.* (Gum) respectively, located in the neighbouring yards to the west of the site. Their TPZs are not encroached by the proposed development. If the ground within their TPZs are kept at existing levels, they are not expected to be impacted.
- 5.18 Tree 23 is a *Viburnum sp.* (Viburnum) growing on the boundary in a neighbouring yard to the west of the subject site. It is of poor health with a thin canopy. Its TPZ of 2.0m is encroached by 43% caused by the roadway of the proposed service station. This is considered a major encroachment (>10%) under *AS4970*. Given its poor health the owner of the tree may be open to an offer of removal and replacement which would alleviate the issue of encroachment. Otherwise, it is recommended the proposed design be altered to lower the encroachment level to at or below 10%. A Non-Destructive Root Investigation can also be performed to determine the presences of root from the tree within the subject site but due to the dense vegetation in the area this may be difficult to conduct.
- 5.19 Tree 24 is a *Corymbia citriodora* (Lemon-scented Gum) street tree located in the nature reserve along Sir Thomas Drive at the southeast corner of the site. Its TPZ of 3.2m is not encroached by the proposed development and it is not expected to be impacted.
- 5.20 Tree Group 25 is a thicket of *Melaleuca ericifolia* (Swamp Paperbark) located close to a drain towards the north-west corner of the site. The thicket is made up of multiple small stems, some of which are located within the subject site. Several of these may require removal to accommodate the driveway of the proposed service station. Based on aerial imagery (Appendix 1) this thicket is more than 10 years old and is covered by *Clause 52.17*. A permit will be required for the removal of any of these trees within the site boundary. Permits will also be required from the Responsible Authority for the removal of any of these trees outside the site boundary.
- 5.21 Tree Group 26 are two *Acacia implexa* (Lightwood) sharing a combined canopy at the north-west corner of the subject site. Their TPZs of 3.1m are not encroached by the proposed development and they are not expected to be impacted.
- 5.22 If any boundary fences are to be replaced, it is recommended that any section of the new fences that pass through the TPZ of trees to be retained be of light-weight construction with post holes hand dug and relocated if necessary to avoid major roots. It is also recommended that all landscape areas within the TPZ of trees to be retained be maintained at existing grades.

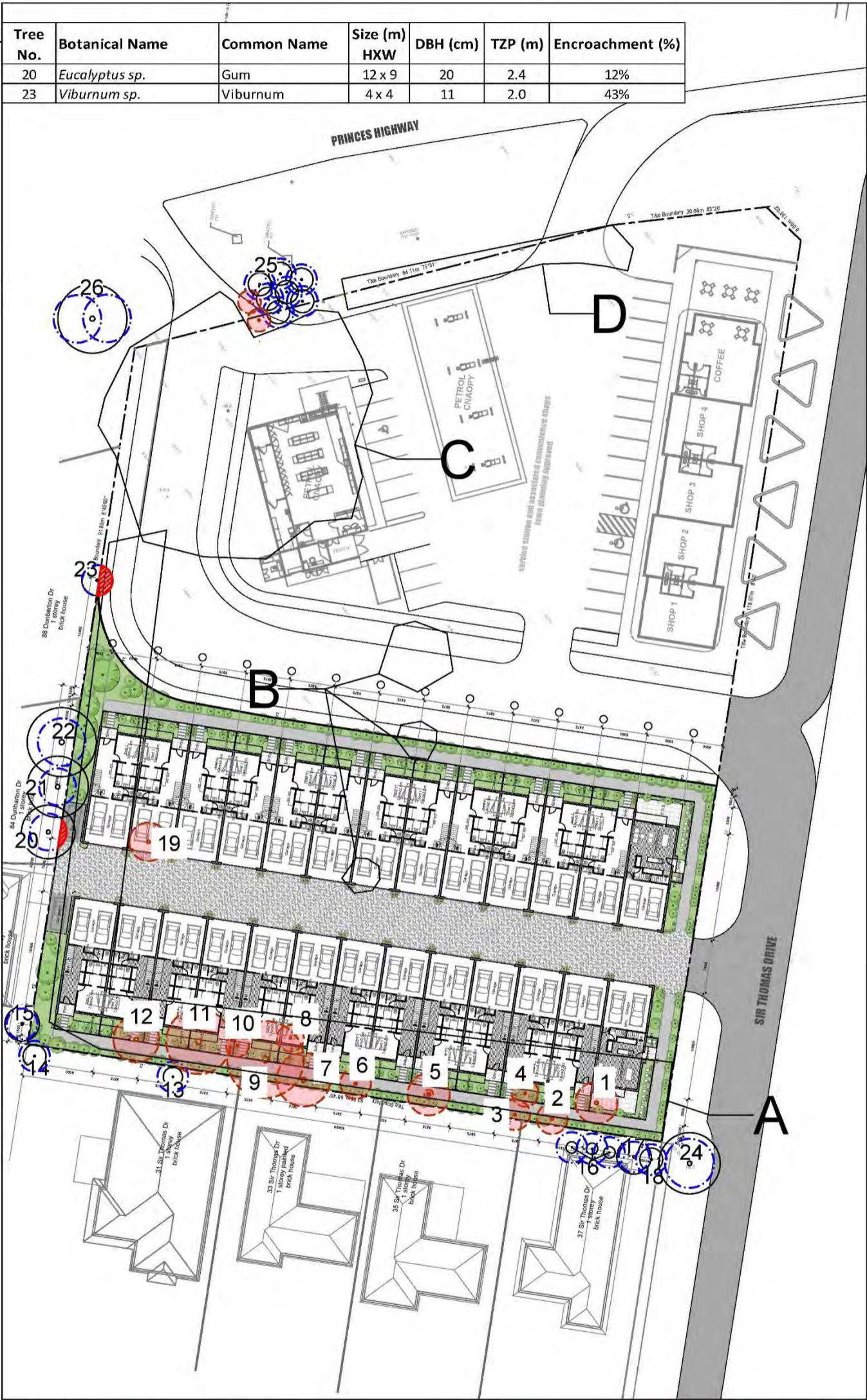
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TPZ IMPACT ASSESSMENT PLAN

LEGEND

- Existing Tree
Blue denotes TPZ
- Existing Tree To Be Removed
- TPZ Encroachment



JOHN PATRICK
LANDSCAPE ARCHITECTS PTY LTD
324 Victoria Street,
Richmond, VIC 3121
T +61 3 9429 4855
F +61 3 9429 8211
admin@johnpatrick.com.au
www.johnpatrick.com.au

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PROJECT
Residential and Commercial Development

ADDRESS
Lot H, Princes Highway, Pakenham

DRAWING
Impact Assessment Plan



SCALE 1:500 @A3
DATE 20-04-2022
DRAWN JH
CHECKED
JOB NO 22-146
DWG NO IAP01

6 Conclusion

- 6.1 A total of 30 trees, tree groups, or areas were assessed, comprising 16 within the subject site and 14 within the road reserve or neighbouring properties.
- 6.2 Site trees 1-13, 19, and Tree Group B will require removal to accommodate the proposed development. These have been assessed as having low arboricultural value.
- 6.3 Trees 1, 3, 7-9, 11, 12, and 19 have been identified as being native to Victoria but less than ten years old and are exempt from *Clause 52.17* due to *52.17-7 Table of exemptions -Regrowth*.
- 6.4 Trees 2 and 4 have been identified as being native to Victoria and are greater than ten years old. They will require a permit for their removal under *Cause 52.17*.
- 6.5 Trees within Area C were inaccessible. They will require removal to accommodate the development. The area will require the removal of blackberries and gorse before a full assessment of the trees can be made. Some trees may be subject to *Clause 52.17* and will require a permit for their removal.
- 6.6 Trees within Area D will require removal to accommodate the proposed development. Based on the size of the trees they are estimated to be less than ten years old and are exempt from *Clause 52.17* due to *52.17-7 Table of exemptions -Regrowth*.
- 6.7 Part of Tree Group 25 will require removal to accommodate the development. This thicket is greater than ten years old and will require a permit for their removal under *Clause 52.17*. Trees outside the site boundary to be removed will require a permit from the Responsible Authority.
- 6.8 Neighbouring Trees 13-18, 21, 22, 24, and 26 are not expected to be impacted by the proposed development.
- 6.9 Neighbouring Tree 20 is encroached by 12%. It is not expected its long-term health will be impacted by the development.
- 6.10 Neighbouring Tree 23 is encroached by 43%. It is recommended the owner be petitioned for the tree's removal and replacement. If this is not practical or accepted, it is recommended the design be altered to lower the encroachment level to less than 10%.

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7 Recommendations

- 7.1 A Tree Management Plan be prepared to direct works around trees to be retained.

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8 Descriptors

Tree Number:

Refers to the identification number for reference purposes, denoted on the Tree Data and Tree Survey Plan.

Botanical Name:

Botanical name of species, based on nomenclature and spelling in Spencer, R 1995, *Horticultural flora of South Eastern Australia* (vols. 1-5), University of NSW Press, Sydney. Where Eucalyptus spp. are not found in this source, nomenclature is based on Euclid: Eucalypts of Australia, 2006, Centre for Australian National Biodiversity Research (CANBR). Eucalypt subspecies information is also based on this source.

While accurate tree identification is attempted, and uncertainties are indicated, some inaccuracies in tree identification may still be present – especially in the case of difficult to determine genera (e.g. *Cotoneaster* and *Ulmus*), and with cultivars which can have similar characteristics.

From time to time taxonomists revise plant classification, and name changes are assigned. If it is known names have been revised post the publication of the relevant above listed source, the new nomenclature has been used.

Common Name:

Common names are based primarily on names and spelling used by Spencer in Horticultural Flora of South Eastern Australia (vols 1-5). The source of common names is taken in the following order:

- Single name supplied in Horticultural Flora of South Eastern Australia;
- First in list of names supplied in Horticultural Flora of South Eastern Australia, unless another name in the list is deemed more appropriate;
- Common name as per Costermans, LF 2006, *Trees of Victoria and adjoining areas*; Costermans Publishing, Victoria.
- Most widely used common name if not available in either source previously mentioned.

Common names are provided for thoroughness; the botanical name should be used when referring to the tree taxon.

Age:

Juvenile: Tree has recently been planted and is still in establishment phase. Tree currently makes little contribution to the amenity of the landscape. Trees of this age are possible candidates for relocation during development.

Semi-mature: Tree has established but has not yet developed mature habit. The tree provides some landscape contribution. Tree size would still be expected to increase considerably provided there are no significant changes to existing growing conditions.

Maturing: Tree has developed mature structural habit but has substantial potential to increase in size.

Mature: Tree has or is close to reaching full potential and expected size. Growth rate has slowed, however the tree does not exhibit any major signs of health or structural weakness due to age.

Over mature: Tree is no longer actively putting out extension growth, and is starting to show signs of decline in health due to age. Canopy may thinning and signs of die back in the canopy may be present

Height: The tree's height in metres

Width: The tree's average canopy width in meters. Variations in canopy width to that stated may be present due to canopy asymmetry.

DBH: The tree's trunk Diameter at Breast Height. Measured at 1.4m above ground level, in accordance with *AS4970 Protection of trees on development sites*, unless specified as having been measured lower. DBH may be estimated or measured, as specified in the report. In the case of multi-stemmed trees, stem diameter is either listed individually, or a measurement taken at a point lower than the point of stem divergence. In some cases, especially where trees are not considered worthy of retention or stems are too numerous the DBH may simply be listed as 'multi-stemmed'.

Health:

Good: Tree is not stressed and shows no obvious signs of pest or disease. It is free of wounding. Annual growth rate is as would be expected of a healthy specimen in the same area. There are no signs of die back and canopy is dense. Tree maybe partially suppressed by neighbouring trees.

Fair: Tree is showing signs of reduced health. It maybe drought stressed or show partial signs of pest or disease. Foliage density is less than optimal and minor die back may be present. Tree is typical of its species. Remedial works may improve tree health.

Poor: Tree exhibits signs of stress, e.g. sparse canopy and possibly stunted growth. A large number of dead branches or dieback are present. Tree is likely to be significantly affected by pests or disease. Tree often in decline. Remedial works not expected to improve long-term health.

Dead: Tree shows no signs of life and is not growing.

Note on Deciduous Species: Assessment of deciduous species can be problematic and results may vary depending on the time of year. Descriptor comments in relation to foliage density do not apply to deciduous trees assessed when dormant or entering or exiting dormancy. Time of leaf drop or bud burst and extent of bud swell may be considered in the health rating of these trees.

The ratings indicate that certain characteristics listed have, or have not, been observed. Inspections do not assess the entire tree in detail for each characteristic. The comments category should be referred to for further information.

Structure:

As a rule, the structure rating is based on identified faults in tree habit which reduce the structural integrity and may lead to partial or entire tree failure. It must be noted, however, that this is not a full hazard or failure assessment.

Good: Tree appears to have no obvious structural defects which would diminish the tree's structural integrity.

Fair: The tree has one or more obvious structural defects. e.g. dead branches or codominant stems, however the observed defects are unlikely to prevent retention of the tree. Judicious remedial intervention could remove structural defects and improve the structure rating.

Poor: Tree has at least one or more structural defects that remedial intervention cannot rectify without significantly reducing the retention value of the tree. These defects reduce the useful life expectancy of the tree.

Hazardous: The tree shows one or more structural faults that are prone to failure and present an immediate safety concern. Judicious intervention to remove structural faults and reduce safety risk would leave a tree not worthy of retention. These trees should be removed as a high priority.

Arboricultural Value:

The Arboricultural Values shown in the table below are based on the ULE of the tree which considers structure and health ratings and landscape contribution.

The arboricultural value assists in determining the positioning of structures and infrastructure outside the tree's identified TPZ.

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ULE	Landscape Significance			
	High	Medium	Low	Very Low
20+ yrs.	High Arboricultural	Medium Arboricultural Value	Low	Very Low
10-20 yrs.	Medium Arboricultural Value			
5-10 yrs.				
0-5 yrs.	Low Arboricultural Value	No Arboricultural Value		
0 yrs.	No Arboricultural Value			

ULE: The Useful Life Expectancy of the tree from a health, structure, amenity and weediness viewpoint given no significant changes to the current situation occur. This category is difficult to determine, and should be taken as an estimate only. In addition, factors not observed at the time of inspection can lead to tree decline.

- 0 yrs.: Tree should be removed due advanced decline/ dead or hazardous.
- 0-5 yrs. Tree is in decline and has poor health or structural faults which cannot be resolved by intervention. Tree is often over- mature.
- 5-10yrs. Tree of fair health or structure
- 10-20. Semi-mature or mature tree of fair health and structure
- 20+ yrs. Juvenile or semi-mature, or a long lived species of good health and structure.

TPZ (Tree Protection Zone):

The Tree Protection Zone of the tree, measured as a radial distance in metres from the centre of the trunk. The TPZ is calculated using the method specified in Australian Standard AS4970-2009 *Protection of trees on development sites*. $12 \times \text{DBH} = \text{TPZ}$

Recommendation:

i.e. Further exploratory root investigation, alterations to proposed works to allow tree retention.

Comments:

Any additional comments specific to individual tree specimens.

AS4970-2009:

The recognised Australian Standard for the 'Protection of Trees on Development Sites'. It provides guidelines on tree protection and formulas for calculating Tree Protection Zones (TPZs), Structural Root Zones (SRZs) and the Diameter at Breast Height (DBH).

AS-4373-2007:

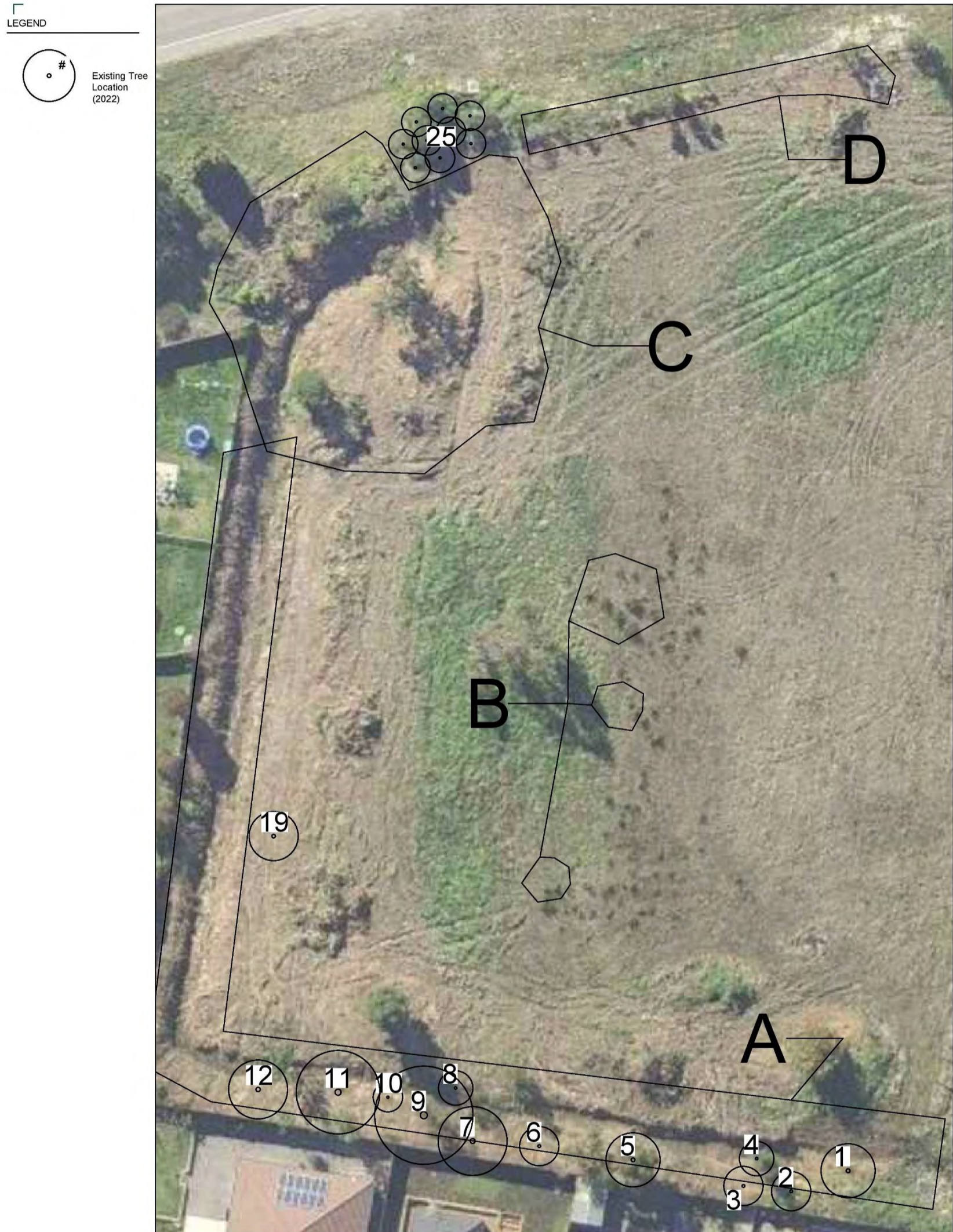
The recognised Australian Standard for the 'Pruning of Amenity Trees'. This Standard provides guidelines on tree pruning to encourage good health and structure.

Ecological Vegetation Class (EVC):

A type of native vegetation classification that is described through a combination of its floristics, life form and ecological characteristics, and through an inferred fidelity to environment attributes. Each EVC includes a collection of floristic communities (i.e. lower level in the classification that is based solely on groups in the same species) that occur across a biogeographic range, and although differing in species, have similar habitat and ecological processes operating.

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9 Appendix 1: Tree Location Plan - 2013



JOHN PATRICK
LANDSCAPE ARCHITECTS PTY LTD
324 Victoria Street,
Richmond, VIC 3121
T +61 3 9429 4855
F +61 3 9429 8211
admin@johnpatrick.com.au
www.johnpatrick.com.au

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PROJECT
Residential and Commercial Development
ADDRESS
Lot H, Princes Highway, Pakenham

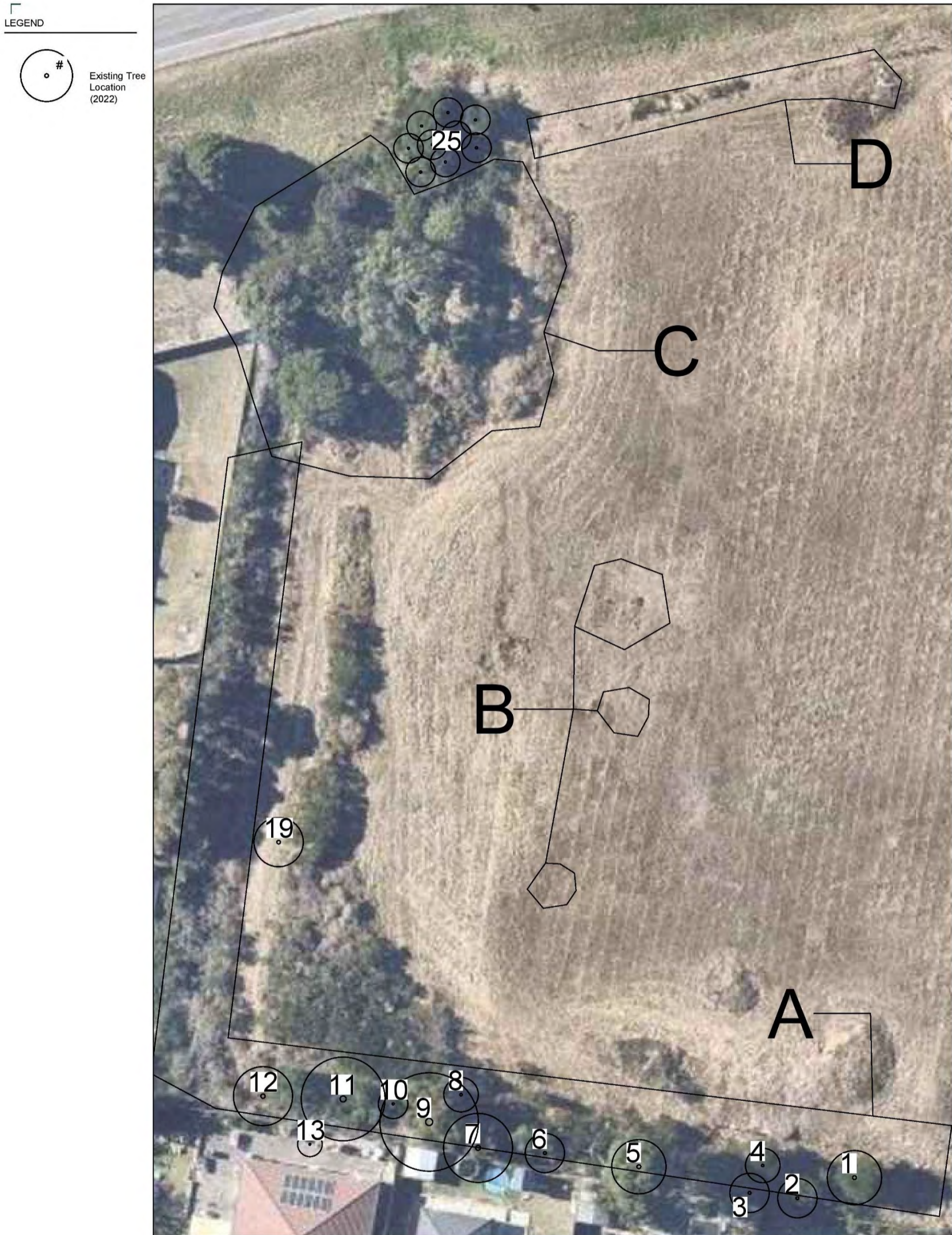
DRAWING
Tree Location Plan- 2013



SCALE 1:350 @A3
DATE 20-04-2022
DRAWN JH
CHECKED
JOB NO 22-146
DWG NO TLP02

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10 Appendix 2: Tree Location Plan – 2015



<p>JOHN PATRICK LANDSCAPE ARCHITECTS PTY LTD 324 Victoria Street, Richmond, VIC 3121 T +61 3 9429 4855 F +61 3 9429 9211 admin@johnpatrick.com.au www.johnpatrick.com.au</p>	CLIENT	PROJECT	DRAWING	SCALE
	J T Pakenham Pty Ltd	Residential and Commercial Development	Tree Location Plan - 2015	1:350 @A3
	COPYRIGHT	ADDRESS		DATE
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				DRAWN
			JH	CHECKED
				JOB NO
				22-146
				DWG NO
				TLP03

17 May 2022



Attention: 

Lot H Princes Highway, Pakenham

Transport Impact Assessment

Dear 

onemilegrid has been requested by Valeo Construction to undertake a Transport Impact Assessment of the proposed residential development at Lot H Princes Highway, Pakenham.

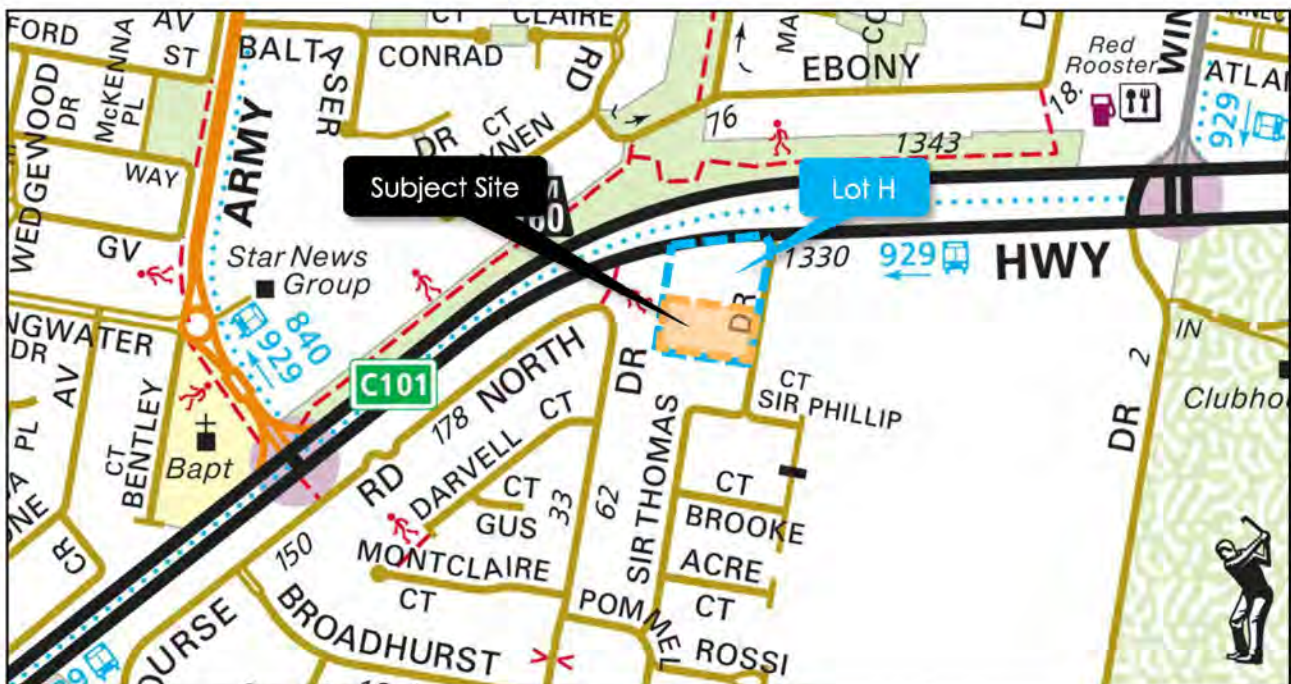
As part of this assessment the subject site has been inspected with due consideration of the development proposal, and relevant background reports have been reviewed.

EXISTING CONDITIONS

Site Location

The subject site is located on the south-western corner of the intersection between Princes Highway and Sir Thomas Drive, and is currently addressed as Lot H Princes Highway, Pakenham, as shown in Figure 1.

Figure 1 Site Location



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The subject application pertains to the southern portion of the site only, as the northern portion has been earmarked for a service station and retail development as part of a separate application. Further discussion on this is provided below.

The subject site is generally quadrilateral in shape, and contains a road frontage of approximately 50 m to Sir Thomas Drive. The site covers a total area of approximately 400 m².

The site is currently vacant with no formal vehicle access provided onto the site.

Land use in the immediate vicinity of the site is residential in nature, and includes commercial uses to the north-east, and low-density residential uses to the east. It is noted that the Pakenham Golf Club is located to the southwest of the site.

An aerial view of the subject site is provided in Figure 2.

Figure 2 Site Context (4th February 2022)



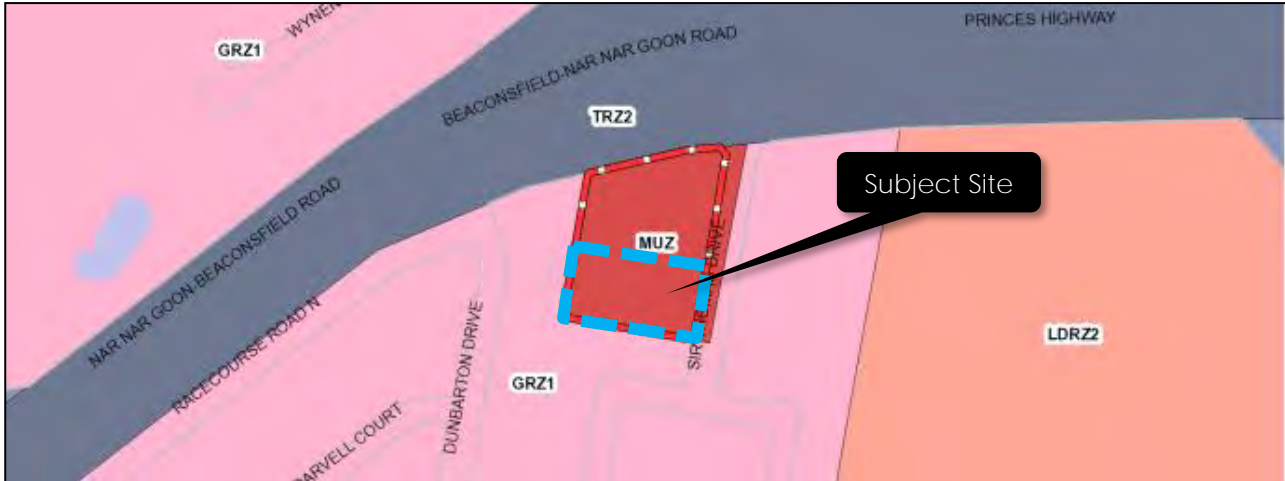
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Planning Zones and Overlays

It is shown in Figure 3 that the site is located within a Mixed Use Zone (MUZ), for which the permitted uses are listed in Clause 32.04 in the Cardinia Planning Scheme.

Figure 3 Planning Scheme Zones



Additionally, the entire site abuts Princes Highway, which is within a Transport Zone (TRZ2); Principal Road Network.

Road Network

Princes Highway

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Princes Highway is an arterial road extending south-east from Melbourne CBD, linking with the Princes Freeway approximately 5 km to the east of the site. Princes Highway provides two traffic lanes in each direction divided by a central median, within the vicinity of the site.

The cross-section of Princes Highway at the frontage of the site is shown in Figure 4.

Figure 4 Princes Highway, looking east from adjacent to the subject site



An 80km/h speed limit applies to Princes Highway in the vicinity of the site.

Sir Thomas Drive

Sir Thomas Drive is a local road generally aligned north-south, running between Princes Highway in the north, and Pommel Street in the south. Sir Thomas Drive facilitates two-way movements in each direction.

The cross-section of Sir Thomas Drive at the frontage of the site is shown in Figure 5.

Figure 5 Sir Thomas Drive, looking south from adjacent to the subject site



The default 50km/h speed limit applies to Sir Thomas Drive in the vicinity of the site.

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Sustainable Transport

The full public transport provision in the vicinity of the site is shown in Figure 6 and detailed in Table 1.

Figure 6 Public Transport Provision

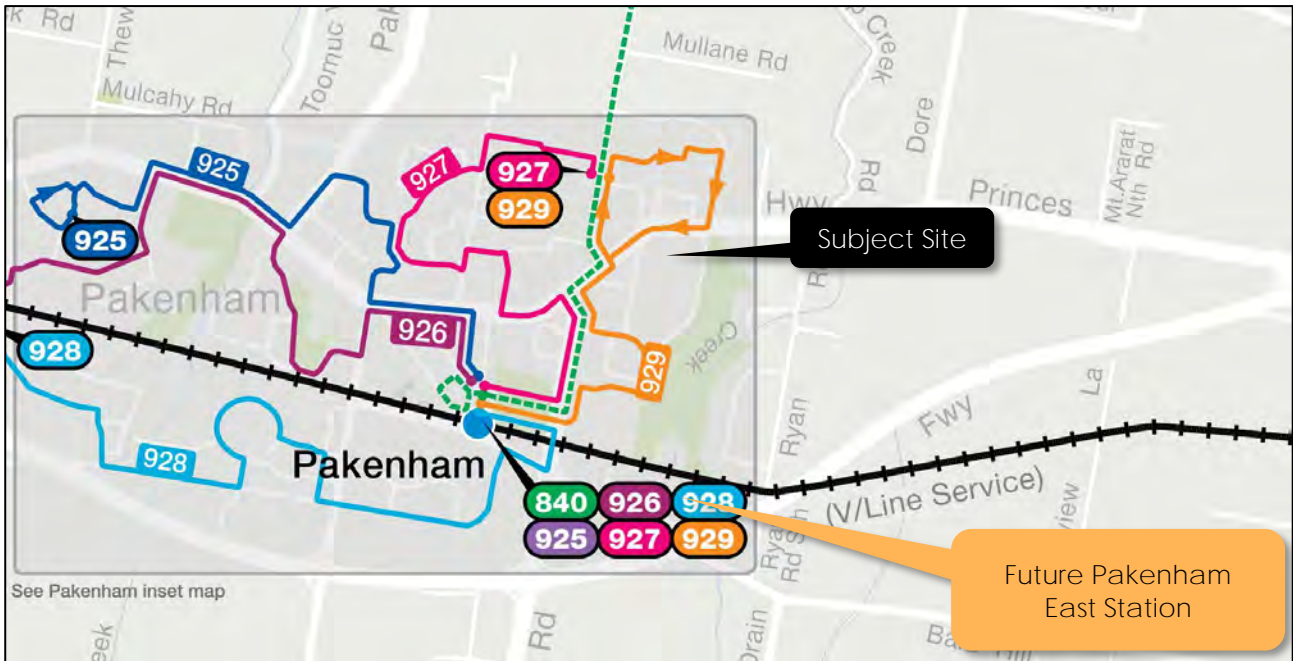


Table 1 Public Transport Provision

Mode	Route No.	Route Description	Nearest Stop/Station
Train		Pakenham Line	Pakenham Station
Bus	840	Gembrook - Pakenham via Pakenham Upper	Princes Hwy / Army Rd
	927	Pakenham Station - Pakenham North via Meadowvale	Princes Hwy / Racecourse Rd
	929	Pakenham Station - Pakenham North via Army Rd & Windermere Blvd	Princes Hwy / Army Rd

The site has limited access to sustainable transport modes, with only bus routes 840, 927 and 929 operating within walking distance of the site. Rail services, and additional bus services are accessible from Pakenham Activity Centre, approximately 1.9 kilometres to the south-west.

It is noted that additional services are anticipated to be brought online as the area to the east of the site continues to develop in accordance with the Pakenham East Precinct Structure Plan. This includes the establishment of Pakenham East Railway Station, estimated to be constructed and operational in 2025, for which the proposed location is shown above in Figure 6.

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DEVELOPMENT PROPOSAL

It is proposed to develop the subject site for the purposes of a residential development, comprising 26 three-bedroom townhouses.

Vehicular access will be provided via a double-width crossover from Sir Thomas Drive with all parking accessible from a central accessway.

Each townhouse will be provided two car parking spaces within individual double garages situated on the ground floor. Additionally, two visitor car parking spaces are provided at the frontage of the site.

The proposed development includes pedestrian access to all townhouses via footpaths situated to the north and south of the townhouses. These footpaths will connect to the western footpath along Sir Thomas Drive, for enhanced pedestrian connectivity.

Two communal bin storage areas are provided for the development at the frontage of the site, which will provide storage space for development generated waste prior to bin collection.

The modifications to traffic movements as part of the service station development will result in redistribution of northbound movements via the new access road.

As mentioned earlier, the proposed development only contemplates the development of residential dwellings at the southern portion of the site, as the northern portion is subject to a separate application.

DESIGN ASSESSMENT

Cardinia Planning Scheme – Clause 52.06

onemilegrid has undertaken an assessment of the car parking layout and access for the proposed development with due consideration of the Design Standards detailed within Clause 52.06-9 of the Planning Scheme. A review of those relevant Design Standards is provided in the following section.

Design Standard 1: Accessways

A summary of the assessment for Design Standard 1 is provided in Table 2.

Table 2 Clause 52.06-9 Design Assessment – Design Standard 1

Requirement	Comments
Be at least 3 metres wide.	Satisfied – minimum width of accessway is 5.4 metres
Have an internal radius of at least 4 metres at changes of direction or intersection or be at least 4.2 metres wide.	N/a – no change in direction along accessway
Allow vehicles parked in the last space of a dead-end accessway in public car parks to exit in a forward direction with one manoeuvre.	N/a – private car park
Provide at least 2.1 metres headroom beneath overhead obstructions, calculated for a vehicle with a wheel base of 2.8 metres.	Satisfied – No overhead obstructions along the accessway

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Requirement	Comments
If the accessway serves four or more car spaces or connects to a road in a Transport Zone 2 or Transport Zone 3, the accessway must be designed so that cars can exit the site in a forward direction.	Satisfied
Provide a passing area at the entrance at least 6.1 metres wide and 7 metres long if the accessway serves ten or more car parking spaces and is either more than 50 metres long or connects to a road in a Transport Zone 2 or Transport Zone 3.	Satisfied
Have a corner splay or area at least 50 per cent clear of visual obstructions extending at least 2 metres along the frontage road from the edge of an exit lane and 2.5 metres along the exit lane from the frontage, to provide a clear view of pedestrians on the footpath of the frontage road. The area clear of visual obstructions may include an adjacent entry or exit lane where more than one lane is provided, or adjacent landscaped areas, provided the landscaping in those areas is less than 900mm in height.	Generally Satisfied - Parking of vehicles within visitor spaces may impede sight distances to pedestrians, it is recommended to reduce the width of the driveway to 6.1m at the boundary
If an accessway to four or more car parking spaces is from land in a Transport Zone 2 or Transport Zone 3, the access to the car spaces must be at least 6 metres from the road carriageway.	N/a – does not connect to a Transport Zone

Design Standard 2: Car Parking Spaces

All double garage spaces are designed with a minimum length of 6.0 metres and a minimum width of 5.6 metres, which exceeds the requirements of the Planning Scheme.

The two visitor car spaces on-site are proposed with a minimum width of 2.6 metres, length of 4.9 metres and are accessed from a 7.0-metre-wide aisle. There for the visitor car parking spaces also exceed the requirements of the Planning Scheme.

To validate the proposed car parking and accessway design, onemilegrid has prepared swept path diagrams demonstrating access to and from critical garages with an 85th percentile passenger vehicle (B85). The swept paths, attached within Appendix A, indicate that all spaces are readily accessible with either a single manoeuvre or correction on entry, and single manoeuvre on exit.

Waste Collection

Two communal bin storage areas are located at the frontage of the site. Residents will transfer waste from their house to the bin storage rooms.

BICYCLE PARKING

Clause 52.34 of the Cardinia Planning Scheme does not specify bicycle parking provision requirements for townhouse style developments, generally assuming that bicycles can be stored in the garage required for each dwelling.

The garage dimensions proposed each exceed the dimension requirements specified under the Planning Scheme, and are therefore considered sufficient for bicycle parking if required.

Regardless, it is considered that visitor bicycle parking could be provided along the pedestrian access if required.

CAR PARKING

Statutory Car Parking Requirements

The car parking requirements for the subject site are identified in Clause 52.06 of the Cardinia Planning Scheme, which specifies the following requirements for the different components of the proposed development.

Table 3 Clause 52.06 – Car Parking Requirements

Use	No/Area	Rate	Car Parking Measure	Total
Dwelling	26	2	to each three or more bedroom dwelling, plus	52
		1	for visitors to every 5 dwellings for developments of 5 or more dwellings	5
Total				57

Based on the above calculations, a total of 57 parking spaces are required for the proposed development.

It is proposed to provide a total of 52 resident car parking spaces and 2 visitor car parking spaces on-site. Although the resident car parking provision is in accordance, there is a shortfall of 3 visitor parking spaces when compared to the Planning Scheme requirements.

Clause 52.06-7 of the Cardinia Planning Scheme indicates that an application to reduce (including reduce to zero) the requirement for car spaces must be accompanied by a Car Parking Demand Assessment. The Assessment must assess the car parking demand likely to be generated by the proposed development.

Car Parking Demand Assessment

It is accepted that the parking requirement of 1 visitor space for every 5 dwellings is in line with the anticipated parking demand for the development, generating demand for up to 5 visitor spaces during peak periods.

With provision for 2 visitor spaces on-site, the proposal generates a shortfall of 3 spaces when compared to the anticipated parking demand.

Clause 52.06-7 of the Cardinia Planning Scheme indicates that a permit may be granted to reduce the number of parking spaces, in consideration of strategic matters, including the availability of car parking.

Review of Car Parking Provision

Site observations, and a review of historical aerial photography in the vicinity suggest that on-street parking demands along Sir Thomas Drive are very low. As such, there is ample on-street parking available to accommodate the modest shortfall in demands generated by the proposal.

Further, it is expected that Sir Thomas Drive will be upgraded along the site frontage to an urban standard, providing formalised kerbside parking for use by visitors. This will provide no less than 35 metres of site frontage for car parking.

In light of this, the proposed parking provisions are considered suitable.

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TRAFFIC

Traffic Generation

It is generally accepted that single dwellings on a lot in outer suburban areas may generate up to 10 vehicle trips per day, whilst in areas with good public transport, and for higher density dwellings, lower traffic generation rates are often recorded. Table 4 provides a summary of the typical traffic generation rates (both peak hour and daily) for a variety of dwelling types.

Table 4 Typical Residential Traffic Generation Rates

<i>Dwelling Type</i>	<i>Peak Hour</i>	<i>Daily</i>
Detached low density	0.8 – 1.0 vph	8 – 10 vpd
Larger medium density townhouses and apartments	0.5 – 0.7 vph	5 – 7 vpd
Smaller medium density townhouses and apartments	0.25 – 0.5 vph	2.5 – 5 vpd
High density apartments (with parking)	0.1 – 0.3 vph	1 – 3 vpd

Considering the size of the proposed dwellings, and the proximity of the site to public transport and amenities, it is anticipated that the proposed development may initially generate up to 7 vehicle trips per day per dwelling. Following full development of the area, including schools, recreational facilities, retail developments and public transport routes, a reduction in traffic generation is expected.

Application of this traffic generation rate to the proposed 26 dwellings equates to a daily traffic generation of 182 vehicle movements, with 18 movements occurring during the AM Peak and PM Peak.

Furthermore, during the AM peak, it is estimated that 70% of the residential traffic will be outbound, while during the PM peak, 60% of the residential traffic will be inbound.

Based on the above rates applied to the 18 peak hour vehicle movements, the anticipated traffic generated by the proposed development is shown in Table 5.

Table 5 Anticipated Traffic Generation

<i>Period</i>	<i>Inbound</i>	<i>Outbound</i>	<i>Total</i>
AM Peak	5	13	18
PM Peak	11	7	18

Traffic Impact

Reviewing the volumes above, it is noted that a maximum of 18 vehicle movements are expected during the peak hours, which equates to less than 1 additional vehicle trip every 3 minutes.

It is expected that 20% of the development generated traffic will be distributed southbound towards Racecourse Road, and the remaining distributed towards Princes Highway via the proposed road through the northern portion of the site.

It is anticipated that the modest increase in traffic volumes at the intersections with Princes Highway will be readily accommodated, noting that Sir Thomas Drive is to be serviced by a left-turn deceleration lane, and an upgraded left-out intersection to the west via the service station access road. A signalised intersection provided at Oaktree Drive to the east will provide sufficient gaps for exiting traffic and fully controlled U-turn opportunities for development generated movements.

Similarly, movements to and from the south will be modest, and once distributed across the numerous feeder routes will be easily absorbed into the surrounding road network, without significant impact to other road users.

CONCLUSIONS

It is proposed to develop the subject site for the purposes of a residential development comprising of 26 three-storey townhouses.

Considering the analysis presented above, it is concluded that:

- The car parking layouts and accesses have been designed generally in accordance with the requirements of the Planning Scheme and are considered appropriate;
- The proposed provision of parking meets the statutory requirements for resident car parking and is considered acceptable;
- The provision of 2 visitor car parking spaces on the site equates to a shortfall of 3 space under the Planning Scheme.
- Sufficient space is provided at the frontage of the site, which can accommodate the shortfall in car parking and therefore provision of parking is considered appropriate;
- The anticipated traffic volumes generated by the development is expected to have minimal impact on the operation of the of the surrounding road network; and
- There are no traffic engineering reasons which would preclude a permit from being issued for this proposal.

Please do not hesitate to contact the undersigned, or Heshan Somaratne on (03) 9982 9747 or at heshan.somaratne@onemilegrid.com.au, should you wish to discuss the above.

Yours sincerely



Associate

onemilegrid

m: 0481 110 642

d: (03) 9982 9717

e: james.dear@onemilegrid.com.au



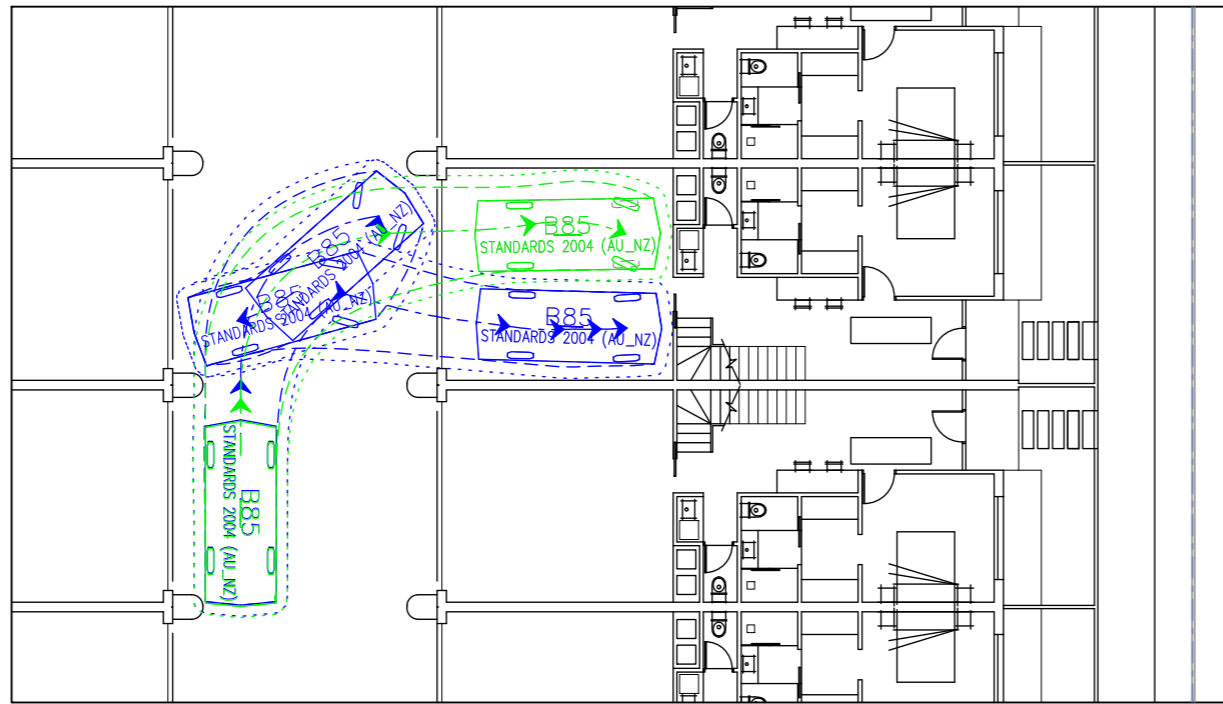
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Appendix A Swept Path Diagrams

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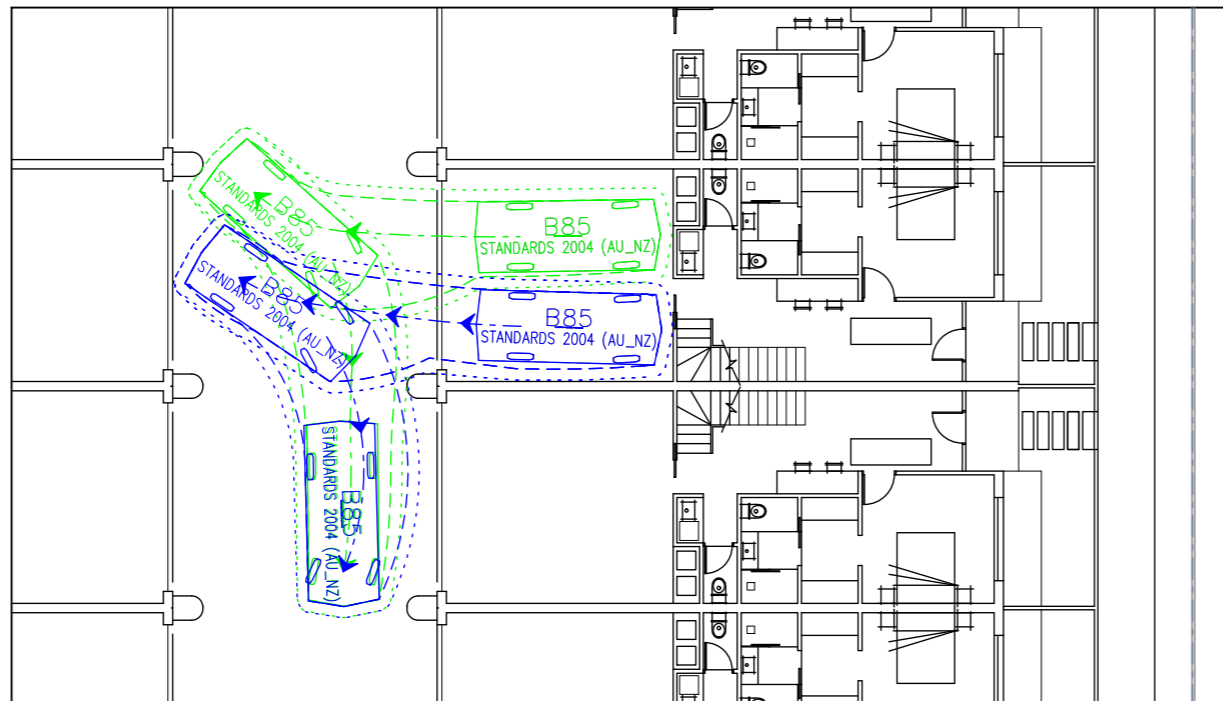
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ENTRY MANOEUVRES

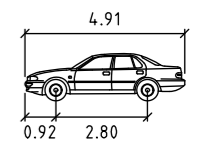
----- DESIGN VEHICLE SWEEP PATHS SHOWN DASHED
 300mm CLEARANCE ENVELOPE SHOWN DOTTED

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EXIT MANOEUVRES

----- DESIGN VEHICLE SWEEP PATHS SHOWN DASHED
 300mm CLEARANCE ENVELOPE SHOWN DOTTED



B85		units
Width	: 1.87	meters
Track	: 1.77	
Lock to Lock Time	: 6.0	
Steering Angle	: 34.1	

CAD File: N:\Projects\22022\220184\Drawings\220184SPA100.dgn

Date Plotted: 16-05-2022 15:02:31



Scale 1:200 @ A3

Drawing Title		
LOT H PRINCES HIGHWAY, PAKENHAM TYPICAL GARAGE ACCESS/EGRESS SWEEP PATH ANALYSIS		
Designed DK	Approved JD	Metway Ref 317 J5
Project Number 220184	Drawing Number SPA100	Revision A



Leigh Design

waste management plans for all urban developments

Leigh Design Pty Ltd

ABN 37 139 522 437

PO Box 115

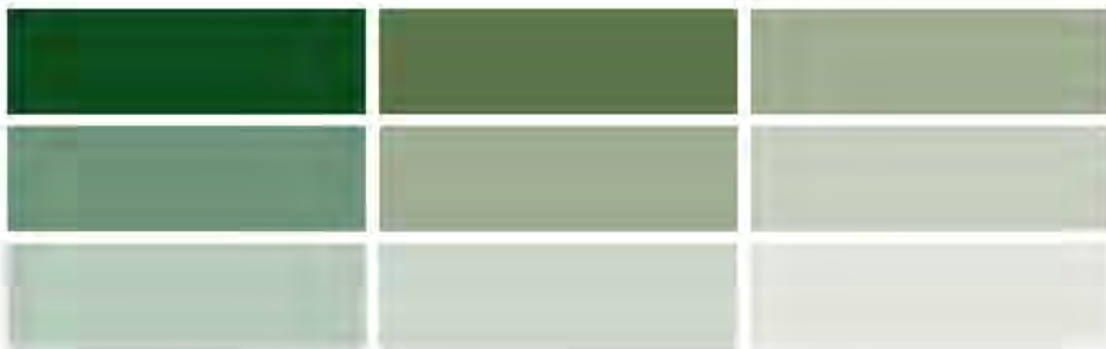
Carnegie VIC 3163

P +61 3 9958 0800

E leo@leighdesign.com.au

I www.leighdesign.com.au

Waste Management Plan



Proposed Development:

Princes Highway, Pakenham, Victoria

Prepared for:

JT Pakenham Pty Ltd

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Document Control

Report Date: 12 April 2022

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Enclosure: Site Plan with waste facilities	

WASTE MANAGEMENT SUMMARY

- The Operator, as defined below, shall be responsible for managing the waste system and for developing and implementing safe operating procedures.
- Waste shall be stored within the development (hidden from external view).
- Users shall place sorted waste into designated collection bins.
- Waste shall be collected on Sir Thomas Drive. The collection contractor shall transfer bins between the waste areas and the truck.
- A private contractor shall provide waste collection services.

GLOSSARY

Operator: refers to the Owners Corporation, who shall manage site operations (via staff and contractors, if required).

User: refers to residents, who shall utilise the waste system.

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1 SPACE AND SYSTEM FOR WASTE MANAGEMENT

1.1 Development Description and Use

This development shall consist of residential townhouses (refer to Table 1).

1.2 Estimated Waste Generation

The following table summarises the waste estimate (m³/week):

Table 1: Waste Estimate

Waste Source	Base Qty (est.)	Garbage	Recycling
Townhouses	No. of units = 26	3.12	3.12
TOTAL (m³/wk)		3.12	3.12

Note: Waste generation rates are based on Council's volumetric bin allocations.

1.3 Collection Services

The municipal wheelie bin service would be unsuitable due to the insufficient kerbside length for the estimated wheelie bins. Therefore, a private contractor shall be engaged to collect waste. The Operator shall choose a waste collection provider, negotiate a service agreement, and pay for these services.

Note: Every rateable tenement is liable to pay for municipal charges irrespective of the level of collection services provided by Council.

1.4 Location, Equipment, and System Used for Managing Waste

The waste management system is summarised as follows:

- Dwelling receptacles for garbage, recycling, glass, and organics (with option for the latter as a benchtop caddy).
- Two Bin Enclosures located at Ground Level.
- Collection bins (kept within the Bin Enclosures - refer to Table 2).

The various collection waste-streams are summarised as follows:

Garbage: General waste shall be placed in tied plastic bags and stored within bins.

Recycling: Two types of bins shall be provided. One type of bin for glass and a second type for all other recyclables (paper, cardboard, aluminium, steel, and plastics). All recyclables shall be commingled until a glass-only service becomes available.

Organics: Users shall place organic waste into Organics bins. Only certified compostable liners may be considered for bins and caddies, to home standard AS5810-2010 (alternatively, the industrial standard AS4736-2006 could be considered if approved by the waste collector). Excess garden waste from communal areas shall be collected and disposed by the landscape maintenance contractor.

Other Waste Streams: The disposal of hard/electronic/liquid and other wastes (polystyrene, batteries, paint, chemicals and detox items, etc) shall be organised with the assistance of the Operator.

These items shall remain within the development until the Operator arranges a private collection from the subject land in accordance with requirements from the relevant authority. In particular e-waste must not be disposed in landfill.

The following table summarises bin quantity/capacity, collection frequency, and area requirements (based on Table 1):

Table 2: Bin Schedule and Collection Frequency

Waste Source	Waste Stream	Bin Qty	Bin Litres	Collections per Week	Net Area m ²
Whole development (shared bins)	Garbage	4	660	1	4.8
	Food Organics	6	240	1	3.0
	Recycling	4	660	1	4.8
	Recycling - future glass	4	240	1	2.0
	Hard/E-Waste	-	-	At Call	2.0
Net Waste Storage Area (excludes circulation), m²:					16.6

Notes:

- Private bins shall be sourced by the Operator (either purchased from a supplier or leased from the collection contractor).
- Subject to stakeholders' preference/capability (and as built constraints), bin sizes and quantities can be changed.

1.5 Planning Drawings, Waste Areas, and Management of the Waste System

The drawings illustrate sufficient space for onsite bin storage, as required by the above schedule.

Notwithstanding the above, the Operator shall stipulate procedures for effective management of the available space.

1.6 Collection Bin Information

The following bins shall be utilised (see Sect. 4.4 for signage requirements):

Table 3: Bin Details

Capacity (litres)	Height (mm)	Width (across front, mm)	Depth (side on, mm)	Empty Weight (kg)	Average* Gross Weight (kg)
240	1060	585	730	13	45
660	1250	1240	780	43	130

Notes:

- * = Average Gross Weight is based on domestic waste studies (which vary subject to locality and waste-type). Expect greater weight for wet or compacted waste.
- Use the above details as a guide only – variations will occur. The above is based on Sulo plastic (HDPE) flat-lid bins.

Table 4: Cardinia Colour Coding

Bin	Garbage	Commingle Recycling	Green Waste
Lid	Green	Yellow	Lime
Body	Green	Green	Green

Note: For private bins, AS4123.7 bin colours can be adopted. Private bins shall be labelled to identify the waste generator and site address. For glass, Victorian publications illustrate bins with purple lids. For Food Waste / Organics bins, AS 4123.7 bins have a Burgundy lid and a Dark Green or Black body.

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2 ACCESS FOR USERS, COLLECTORS, AND COLLECTION VEHICLES

2.1 User Access to Waste Facilities

Residents shall dispose sorted waste into designated collection bins located within the Bin Enclosures.

2.2 Collection Arrangements and Access to Waste Facilities

- A private contractor shall collect waste on Sir Thomas Drive (site's frontage).
- Collection staff (driver and assistant) shall have access to the Bin Enclosures and transfer bins to the truck and back to the enclosures.
- The waste collection shall be carried-out by rear-lift vehicles (nom. 8.8m long and 4m operational height).

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3 AMENITY, LOCAL ENVIRONMENT, AND FACILITY DESIGN

3.1 Noise Minimisation Initiatives

- Collection bins shall feature rubber wheels for quiet rolling during transfers.
- Waste areas shall meet BCA and AS2107 acoustic requirements.
- Local laws shall be observed for all operations in public and private areas.
- For private services, the hours of waste collections shall be as specified in Council's local laws. Also, Section 5 of the Victorian EPA Noise Control Guideline Publication 1254 (see below) shall be observed to protect the acoustic amenity of the development and surroundings.

Victorian EPA Noise Control Guideline Publication 1254 October 2008 (excerpt)

[Section] 5. Domestic Refuse Collection

The main annoyance produced by domestic refuse collections occurs in the early morning (i.e. before 7:00am). Therefore, if possible, routes should be selected to provide the least impact on residential areas during that time.

Collection of refuse should be restricted to the following criteria:

- Collection occurring once a week should be restricted to the hours: 6am to 6pm Monday to Saturday.
- Collections occurring more than once a week should be restricted to the hours: 7am to 6pm Monday to Saturday.
- Compaction should only be carried out while on the move.
- Bottles should not be broken up at the point of collection.
- Routes which service entirely residential areas should be altered regularly to reduce early morning disturbance.
- Noisy verbal communication between operators should be avoided where possible.

3.2 Litter Reduction and Prevention of Stormwater Pollution

The Operator and residents shall be responsible for:

- Promoting adequate waste disposal into the bins (to avoid waste-dumping).
- Securing the waste areas (whilst affording access to users/staff/contractors).
- Preventing overfilled bins, keeping lids closed and bungs leak-free.
- Abating any site litter and taking action to prevent dumping and/or unauthorised use of waste areas.
- Requiring the collection contractor to clean-up any spillage that might occur when clearing bins.

The above will minimise the dispersion of site litter and prevent stormwater pollution (thus avoiding impact to the local amenity and environment).

3.3 Ventilation, Washing, and Vermin-Prevention Arrangements

Waste areas shall feature:

- Ventilation in accordance with Australian Standard AS1668.
- Tight-fitting doors (all other openings shall have vermin-proof mesh or similar).
- Impervious flooring (also, smooth, slip-resistant, and appropriately drained).

- A graded bin wash area, hosecock, hose, and a suitable floor-waste connected in accordance with relevant authority requirements (alternatively, the Operator shall engage a suitable contractor to wash bins in a mobile bin-wash vehicle). The bin and wash areas may overlap, as stored bins can be moved so that a bin can be washed.

The Operator shall regularly clean waste areas/equipment. Also, access doors and bin-lids shall be kept closed.

3.4 Design and Aesthetics of Waste Storage Areas and Equipment

Waste shall be placed within collection bins and stored in designated onsite areas (hidden from external view). Following waste collection activities, bins shall be returned to the storage areas as soon as practicable.

Waste facilities shall be constructed of durable materials and finishes, and maintained to ensure that the aesthetics of the development are not compromised. These facilities and associated passages shall be suitably illuminated (this provides comfort, safety, and security to users, staff, and contractors). Any access doors shall feature keyless opening from within.

The design and construction of waste facilities and equipment shall conform to the Building Code of Australia, Australian Standards, and local laws.

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4 MANAGEMENT AND SUSTAINABILITY

4.1 Waste Sorting, Transfer, and Collection Responsibilities

Garbage shall be placed within tied plastic bags prior to transferring into the collection bins. Cardboard shall be flattened and recycling containers un-capped, drained, and rinsed prior to disposal into the appropriate bin. Bagged recycling is not permitted.

Refer to Section 2 for waste transfer requirements and collection arrangements.

4.2 Facility Management Provisions to Maintain & Improve the Waste System

The Operator shall manage site operations (refer to the glossary in page 2).

It shall be the responsibility of the Operator and residents to maintain all waste areas and components, to the satisfaction of residents and the relevant authority (residents shall maintain their internal waste receptacles).

The Operator shall ensure that maintenance and upgrades are carried-out on the facility and components of the waste system. When required, the Operator shall engage an appropriate contractor to conduct services, replacements, or upgrades.

4.3 Arrangements for Protecting Waste Equipment from Theft and Vandalism

It shall be the responsibility of the Operator to protect the equipment from theft and vandalism. This shall include the following initiatives:

- Secure the waste areas.
- Label the bins according to property address.
- The private collection contractor shall transfer bins between the waste areas and the truck (bins shall not be placed on the street).

4.4 Arrangements for Bins/Equipment Labelling and Ensuring Users and Staff are Aware of How to Use the Waste System Correctly

- The Operator shall provide appropriate signage for the bins. Signage is available at the following internet address: www.sustainability.vic.gov.au.
- The Operator shall publish/distribute “house rules” and educational material to:
 - Inform users/staff about the waste management system and the use/location of the associated equipment (provide the summary in page 2 of this report).
 - Improve facility management results (lessen equipment damage, reduce littering, and achieve cleanliness).
 - Advise users/staff to sort and recycle waste with care to reduce contamination of recyclables.

4.5 Sustainability and Waste Avoidance/Reuse/Reduction Initiatives

The *Environment Protection Act 1970* includes principles of environment protection and guidance for waste management decision making. Also, the *Sustainability Victoria Act 2005* established Sustainability Victoria as the statutory authority for delivering programs on integrated waste management and resource efficiency.

From a design perspective, the development shall support the acts by providing an adequate waste system with ability to sort waste.

The Operator shall promote the observance of the acts (where relevant and practicable) and encourage users and staff to participate in minimising the impact of waste on the environment. For improved sustainability, the Operator shall consider the following:

- Observe the waste hierarchy in the *Environment Protection Act 1970* (in order of preference): a) waste avoidance, b) reuse, c) recycle, d) recovery of energy, e) treatment, f) containment, and g) disposal.
- Peruse the Sustainability Victoria website: www.sustainability.vic.gov.au.
- Participate in Council and in-house programs for waste minimisation.
- Establish waste reduction and recycling targets; including periodic waste audits, keeping records, and monitoring of the quantity of recyclables found in landfill-bound bins (sharing results with users/staff).

4.6 Waste Management Plan Revisions

For any future appropriate Council request, changes in legal requirements, changes in the development's needs and/or waste patterns (waste composition, volume, or distribution), or to address unforeseen operational issues, the Operator shall be responsible for coordinating the necessary Waste Management Plan revisions, including (if required):

- A waste audit and new waste strategy.
- Revision of the waste system (bin size/quantity/streams/collection frequency).
- Re-education of users/staff.
- Revision of the services provided by the waste collector(s).
- Any necessary statutory approval(s).

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5 SUPPLEMENTARY INFORMATION

- The Operator shall observe local laws and ensure that bins aren't overfilled or overloaded.
- Waste incineration devices are not permitted, and offsite waste treatment and disposal shall be carried-out in accordance with regulatory requirements.
- For bin traffic areas, either level surfaces (smooth and without steps) or gentle ramps are recommended, including a roll-over kerb or ramp. Should ramp gradients, bin weight, and/or distance affect the ease/safety of bin transfers, the Operator shall consider the use of a suitable tug.
- The Operator and waste collector shall observe all relevant OH&S legislation, regulations, and guidelines. The relevant entity shall define their tasks and:
 - Comply with Worksafe Victoria's Occupational Health and Safety Guidelines for the Collection, Transport and Unloading of Non-hazardous Waste and Recyclable Materials (June 2003).
 - Assess the Manual Handling Risk and prepare a Manual Handling Control Plan for waste and bin transfers (as per regulatory requirements and Victorian COP for Manual Handling).
 - Obtain and provide to staff/contractors equipment manuals, training, health and safety procedures, risk assessments, and adequate personal protective equipment (PPE) to control/minimise risks/hazards associated with all waste management activities. As a starting point, these documents and procedures shall address the following:

Task (to be confirmed)	Hazard (TBC)	Control Measures (TBC)
Sorting waste and cleaning the waste system	Bodily puncture. Biological & electrical hazards	Personal protective equipment (PPE). Develop a waste-sorting procedure
Bin manual handling	Sprain, strain, crush	PPE, staff training. Maintain bin wheel-hubs. Limit bin weight. Provide mechanical assistance to transfer bins
Bin transfers and emptying into truck	Vehicular strike, run-over	PPE. Develop a Hazard Control Plan for transfers and collections. Maintain visibility. Use a mechanical bin-tipper
Truck access	Vehicular incident, strike, run-over	PPE. Use a trained spotter. Develop a truck-manoeuving and traffic-control procedure

Note: The above shall be confirmed by a qualified OH&S professional who shall also prepare site-specific assessments, procedures, and controls (refer to Section 6).

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6 CONTACT INFORMATION

Cardinia Shire Council (local council), ph 1300 787 624

JJ Richards & Sons (private waste collector), ph 03 9703 5222

Visy (private waste collector), ph 13 84 79

FJP Safety Advisors (OH&S consultant), ph 03 9255 3660

Warequip (tug supplier – for bin transfers), ph 1800 337 711

Sulo MGB Australia (bin supplier), ph 1300 364 388

One Stop Garbage Shop (bin supplier), ph 03 9338 1411

Note: The above includes a complimentary listing of contractors and equipment suppliers. The stakeholders shall not be obligated to procure goods/services from these companies. Leigh Design does not warrant (or make representations for) the goods/services provided by these suppliers.

7 LIMITATIONS

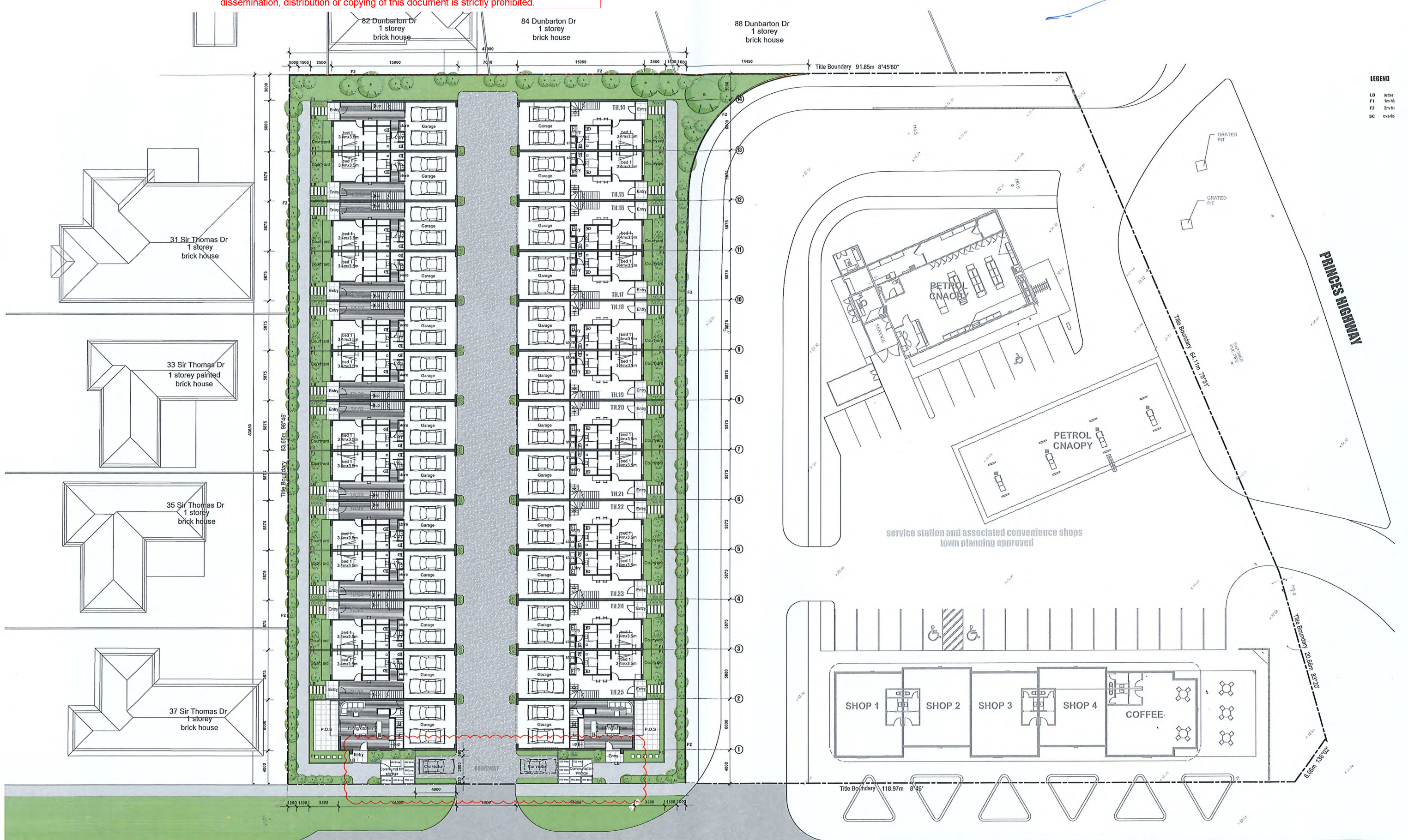
The purpose of this report is to document a Waste Management Plan, as part of a Planning Permit Application.

This report is based on the following conditions:

- Operational use of the development (excludes demolition/construction stages).
- Drawings and information supplied by the project architect.
- The figures presented in this report are estimates only. The actual amount of waste will depend on the development's occupancy rate and waste generation intensity, the user's disposition toward waste and recycling, and the Operator's approach to waste management. The Operator shall make adjustments, as required, based on actual waste volumes (if the actual waste volume is greater than estimated, then the number of bins and/or the number of collections per week shall be increased, STCA).
- This report shall not be used to determine/forecast operational costs, or to prepare feasibility studies, or to document operational/safety procedures.

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LEGEND
 LB border
 F1 1m hi
 F2 2m hi
 SC overb

PRELIMINARY ISSUE

REVISIONS

REV	DATE	DESCRIPTION	initial
A	1/03/22	TP PRELIMINARY ISSUE	NH
B	22/03/22	TP PRELIMINARY ISSUE	NH

initial	REV DATE	DESCRIPTION	initial
NH			
NH			



PROPOSED RESIDENCES

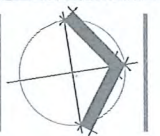
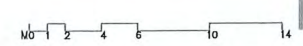
TOWN PLANNING ISSUE

22 MARCH 2022
 DRAWN BY: NH
 CLIENT: VALEO CONSTRUCTIONS

DRAWING:
GROUND FLOOR

SCALE: 200@A1

PRINCES HIGHWAY, PAKENHAM



DRWG NO:
TP 01
 REV: **B**

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Leigh Design

waste management plans for all urban developments

Leigh Design Pty Ltd

ABN 37 139 522 437

PO Box 115

Carnegie VIC 3163

P +61 3 9958 0800

E leo@leighdesign.com.au

I www.leighdesign.com.au

Waste Management Plan



Proposed Development:

Princes Highway, Pakenham, Victoria

Prepared for:

JT Pakenham Pty Ltd

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Document Control

Report Date: 12 April 2022

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DRAWING LIST

TP01	NEIGHBOURHOOD & SITE DESCRIPTION
TP02	NEIGHBOURHOOD & SITE DESCRIPTION
TP03	GROUND FLOOR PLAN
TP04	FIRST FLOOR PLAN
TP05	ROOF PLAN
TP06	ELEVATIONS
TP07	ELEVATIONS & SECTIONS
TP08	SHADOW DIAGRAM - 9am
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TP11	SHADOW DIAGRAM - Midday
TP12	SHADOW DIAGRAM - 1pm
TP13	SHADOW DIAGRAM - 2pm
TP14	SHADOW DIAGRAM - 3pm
TP15	SCREEN DETAIL & FINISHES SCHEDULE
TP16	PERSPECTIVES

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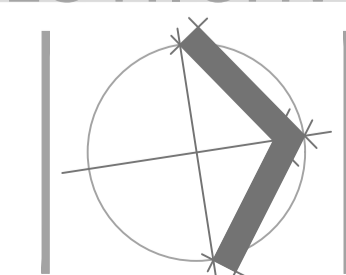
TOWN PLANNING ISSUE

3 MAY 2022
 DRAWN BY: NH
 CLIENT: VALEO CONSTRUCTIONS

DRAWING:

 SCALE: N/A

PRINCES HIGHWAY, PAKENHAM



DRWG NO:
TPOO
 REV: 1



- 1 The subject site is a part of Lot H, Princes Highway, Pakenham, The vacant site has a total site area of 3980 sqm.
- 2 Pakenham Train approx 1.93 km
- 3 Princes Freeway via Racecourse Road approx 2.9 km
- 4 Bus stop routes 929 via Windermere Blvd approx 920m
- 5 Bus stop routes 929 via Barrington Dr approx 950m
- 6 Bus stop routes 925 via Princes Hwy approx 1.9 km
- 7 Pakenham Hills Primary School approx 920m
- 8 Pakenham Primary School approx 870m
- 9 Pakenham Kindergarten approx 2.1 km
- 10 Pakenham Secondary College approx 2.7 km
- 11 Bunnings East Pakenham approx 1.9 km
- 12 VicRoad Pakenham East approx 1.9 km
- 13 Pakenham Central Marketplace approx 2 km
- 14 Cardinia Lakes Shopping Centre approx 800m

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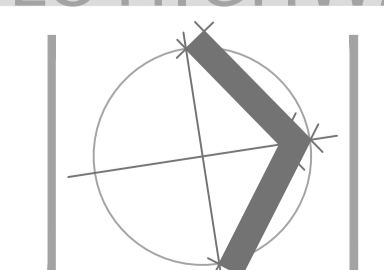
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PROPOSED RESIDENCES

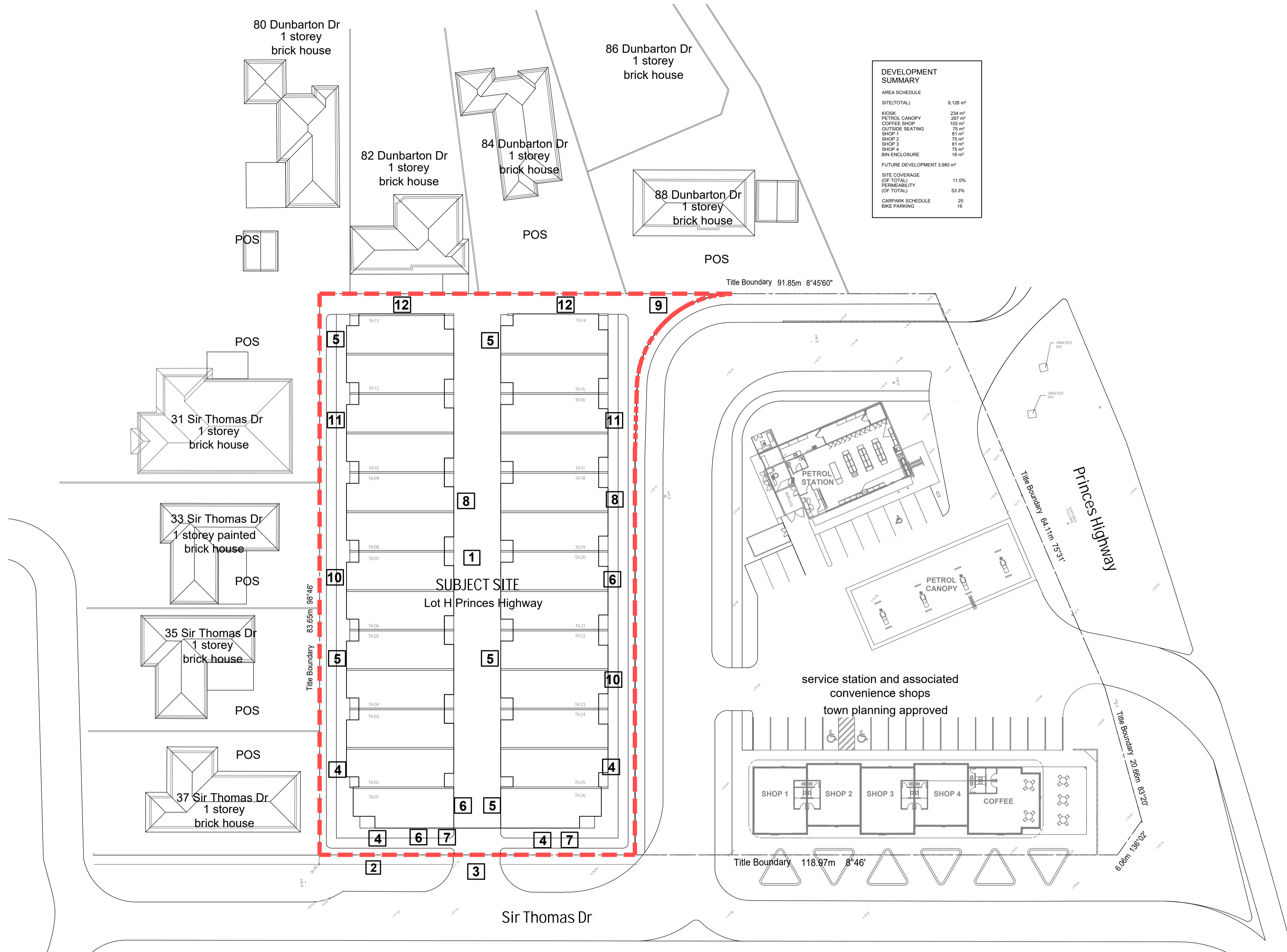
TOWN PLANNING ISSUE
 3 MAY 2022
 DRAWN BY: NH
 CLIENT: VALEO CONSTRUCTIONS

PRINCES HIGHWAY, PAKENHAM

DRAWING:
NEIGHBOURHOOD AND SITE DESCRIPTION
 SCALE: 1:500@A1



DRWG NO:
TPO1
 REV: 1



DEVELOPMENT SUMMARY	
AREA SCHEDULE	
SITE (TOTAL)	9,128 m ²
KIDS	234 m ²
PETROL CANOPY	207 m ²
COFFEE SHOP	103 m ²
OUTSIDE SEATING	75 m ²
SHOP 1	81 m ²
SHOP 2	75 m ²
SHOP 3	81 m ²
SHOP 4	75 m ²
BIN ENCLOSURE	18 m ²
FUTURE DEVELOPMENT	3,980 m ²
SITE COVERAGE (OF TOTAL)	11.0%
PERMEABILITY (OF TOTAL)	53.3%
CARPARK SCHEDULE	25
BIKE PARKING	16

- 1 Subject site is a part of Lot H, Princes Highway, Pakenham. The vacant site has a total site area of 3980 sqm.
- 2 Building setback from Sir Thomas Dr boundary to comply with schedule 1 of clause 32.08 GENERAL RESIDENTIAL ZONE - GRZ1 of Cardinia Planning Scheme
- 3 Proposed new crossover has not impact on existing mature tree and on neighbourhood character
- 4 mailbox provided to each dwelling.
- 5 Screening provided to prevent over looking to adjoining POS. as per clause 55.04-6 standard B22 Rescode
- 6 Selected materials & textures to highlight architectural form and reduce visual bulk.
- 7 Town house front facades orientated to main street which is Sir Thomas Dr and maintain main entry from front street Sir Thomas Dr
- 8 Good cross ventilation and daylight access for all habitable rooms via openable windows
- 9 Maintain all existing mature tree on nature strip
- 10 Building set back to comply with rescode and reduce visual bulk to neighbours at sides.
- 11 Proposed habitable windows are orientated to maximise solar access from the north, north-east and north-west
- 12 Rear setback in accordance with 55.04-1 standard B17 Clause 55 ResCode

AREA SCHEDULE

SITE AREA :	3980SQM
SITE COVERAGE :	2400SQM (60%)
PERMEABILITY :	806SQM (20%)

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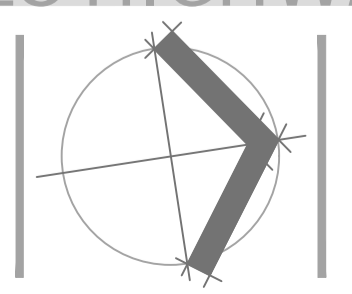
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NH			



PROPOSED RESIDENCES

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DRAWING:
NEIGHBOURHOOD AND SITE DESCRIPTION



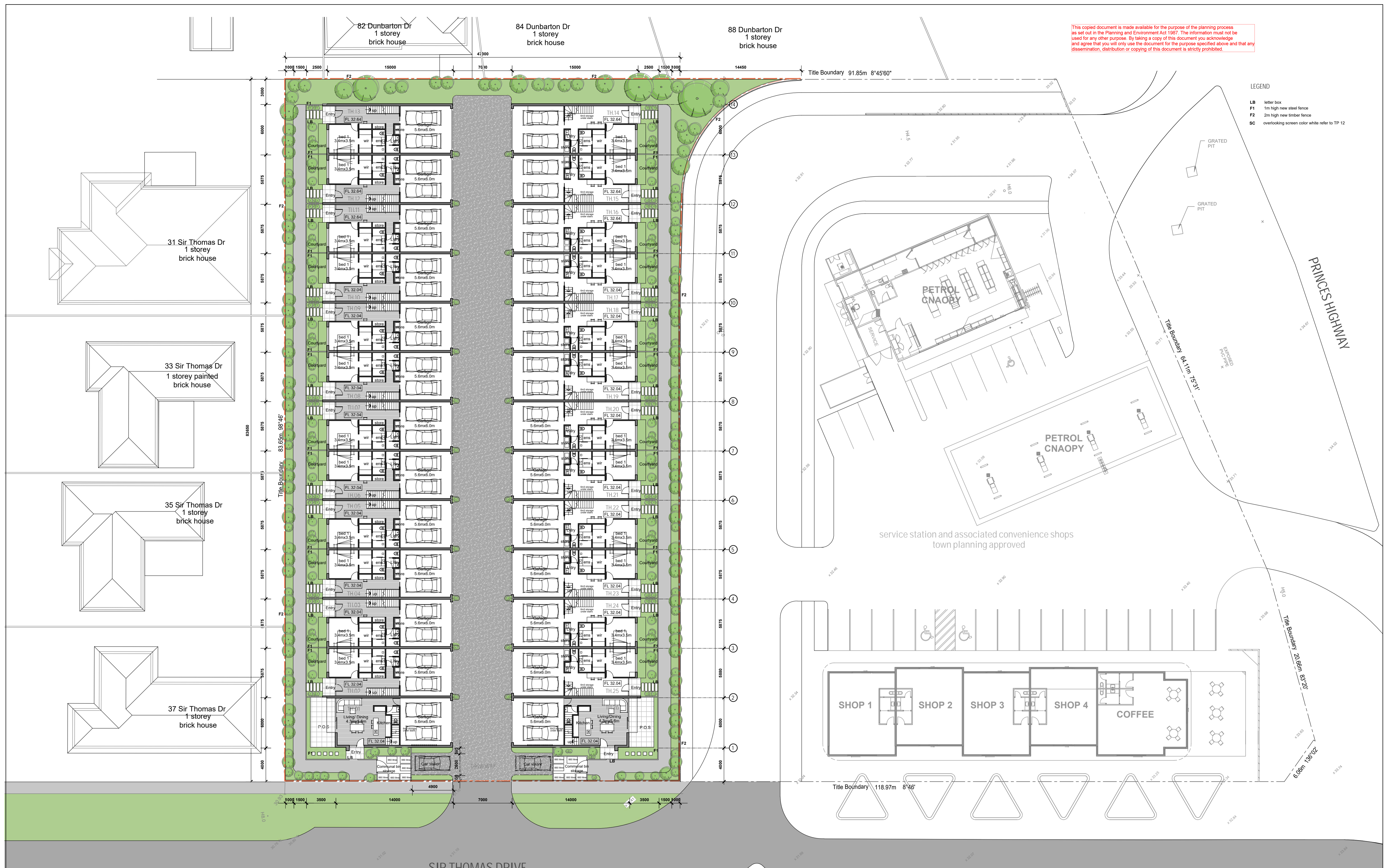
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TPO2
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SCALE: 1:500@A1

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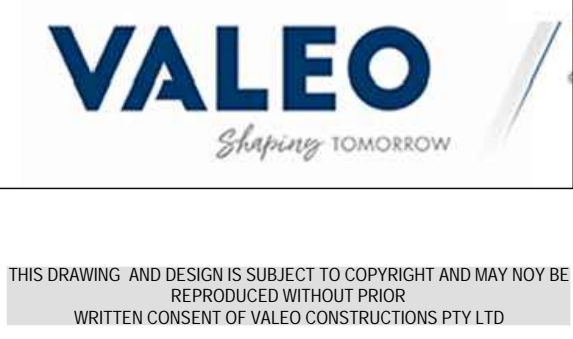
- LEGEND
- LB letter box
 - F1 1m high new steel fence
 - F2 2m high new timber fence
 - SC overlooking screen color white refer to TP 12



REVISIONS

REV	DATE	DESCRIPTION
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initial	REV	DATE	DESCRIPTION
NH			



PROPOSED RESIDENCES
TOWN PLANNING ISSUE
 3 MAY 2022
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DRAWING:
GROUND FLOOR PLAN
 SCALE: 200@A1

PRINCES HIGHWAY, PAKENHAM

DRWG NO:
TPO3
 REV: 1



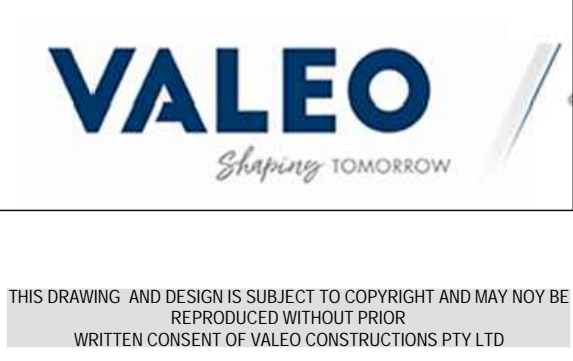
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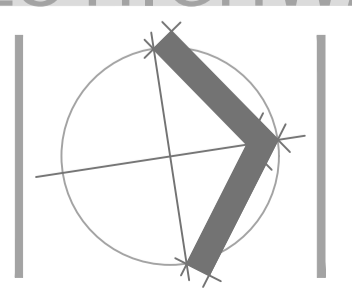
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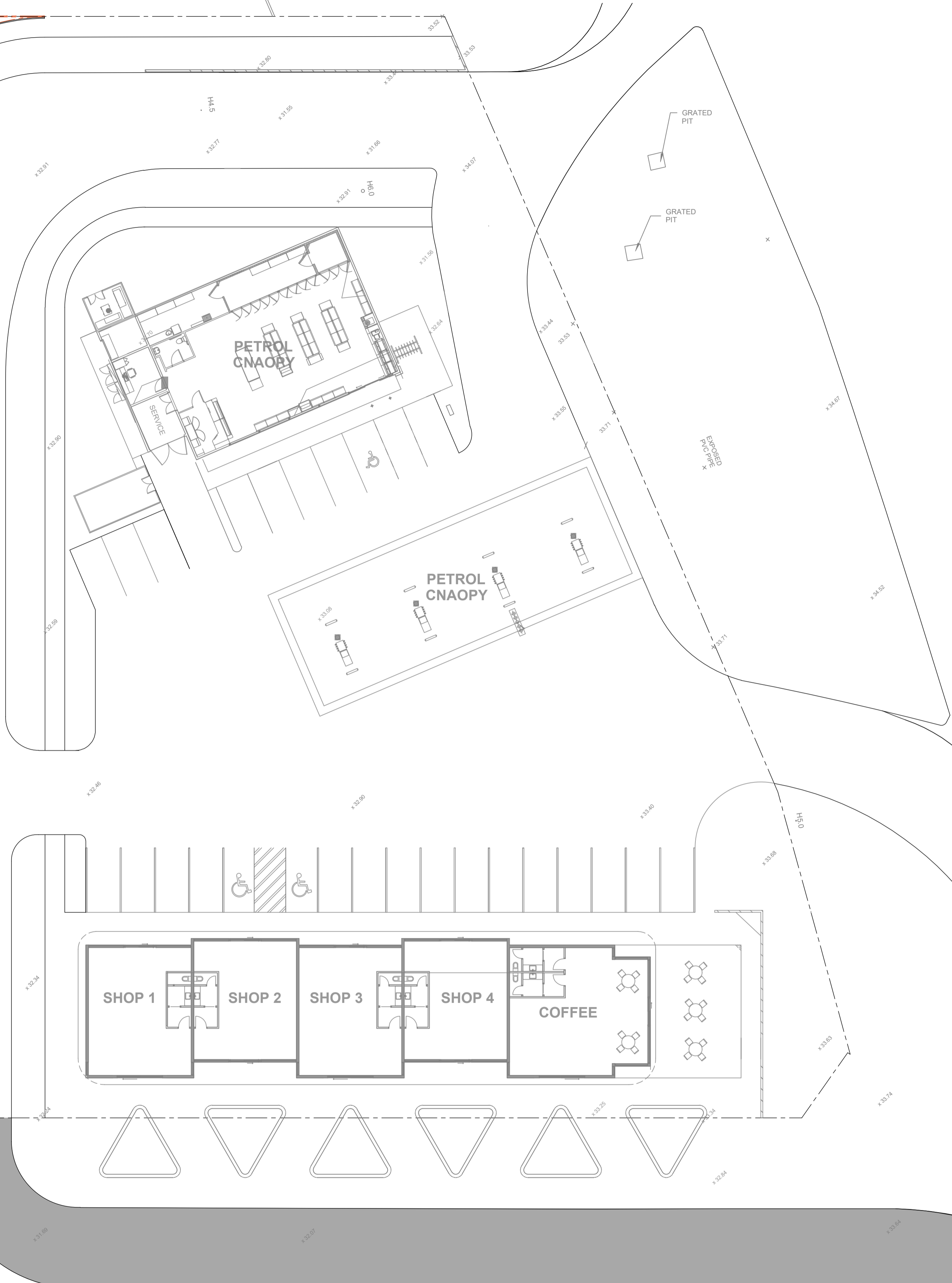
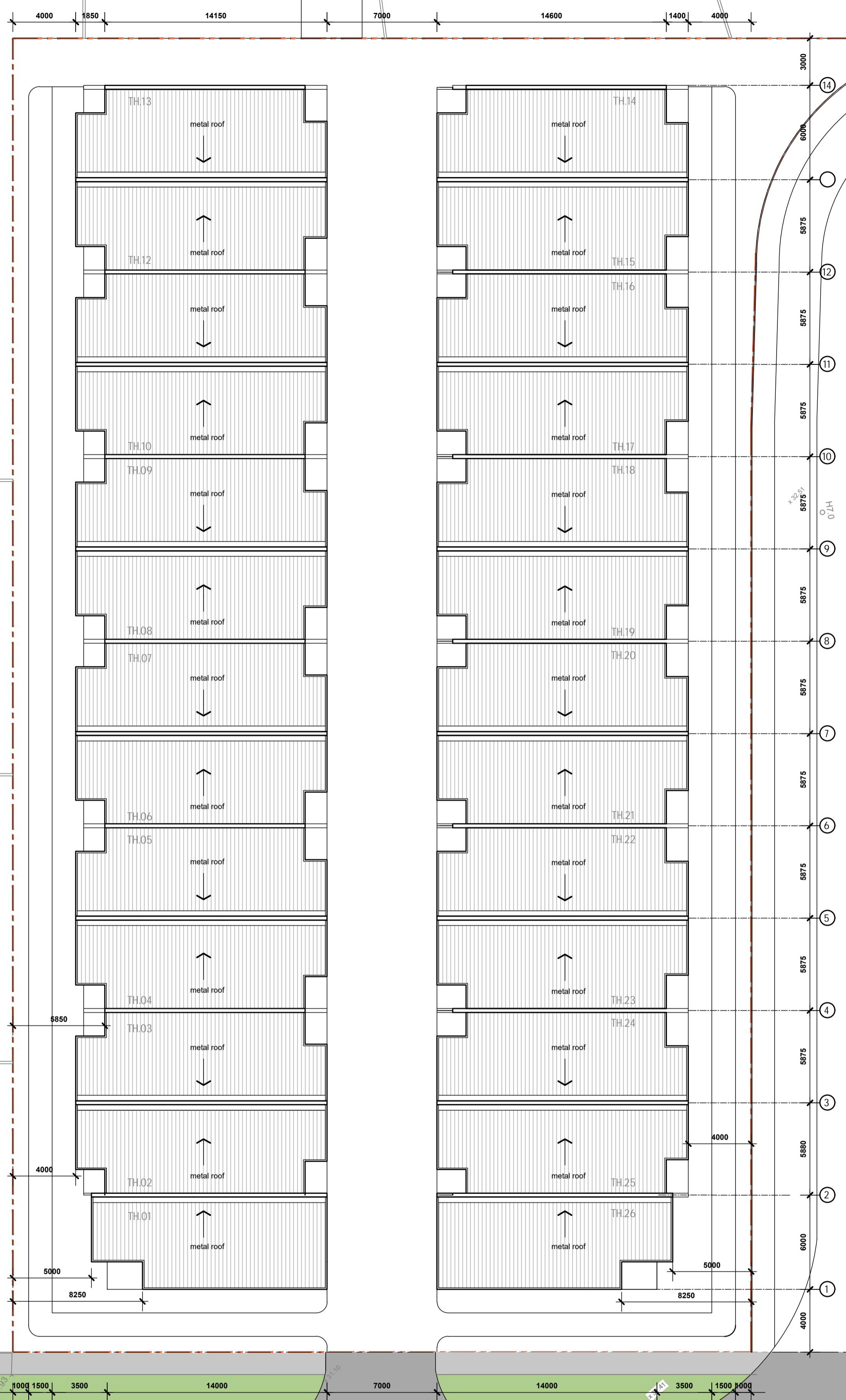
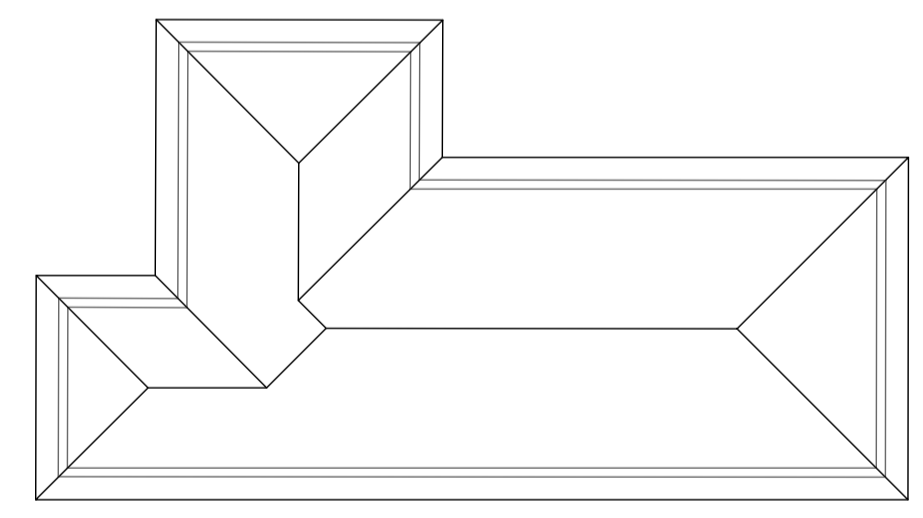
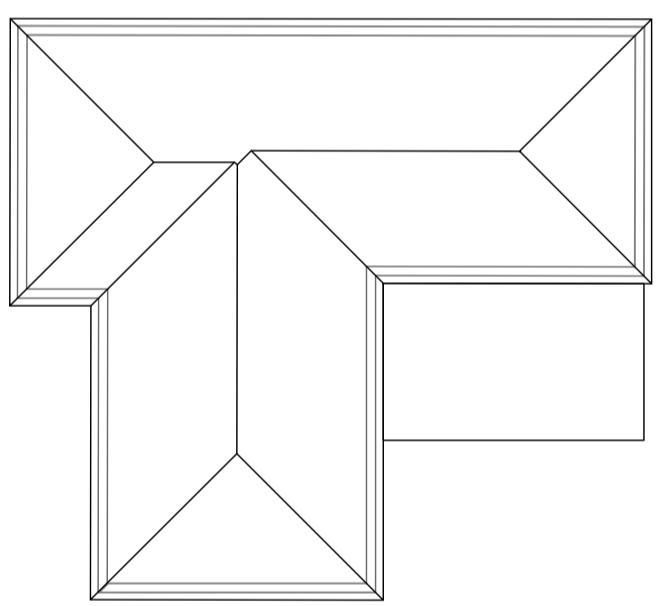
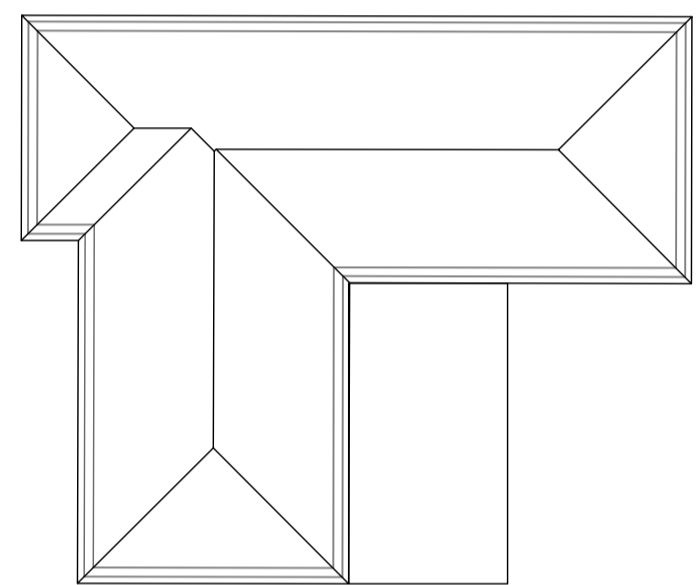
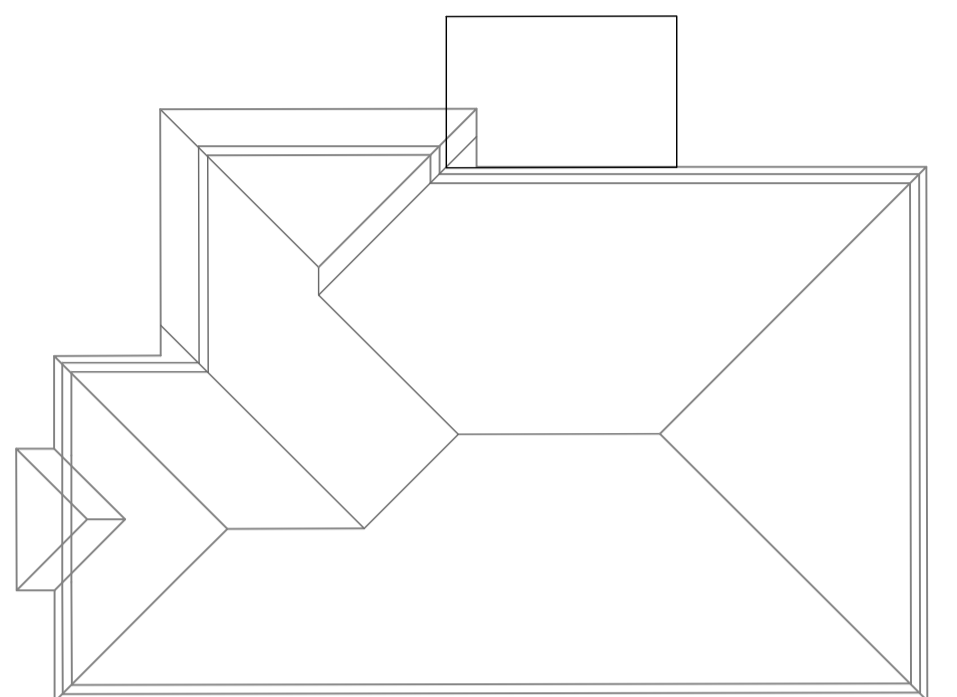
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FIRST FLOOR PLAN
 SCALE: 200@A1

PRINCES HIGHWAY, PAKENHAM
 DRWG NO:
TPO4
 REV: 1



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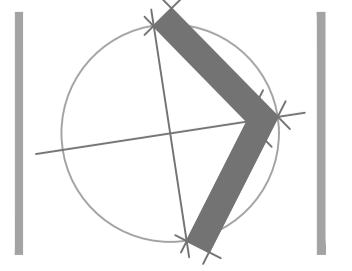
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CLIENT: VALEO CONSTRUCTIONS

DRAWING:
ROOF PLAN

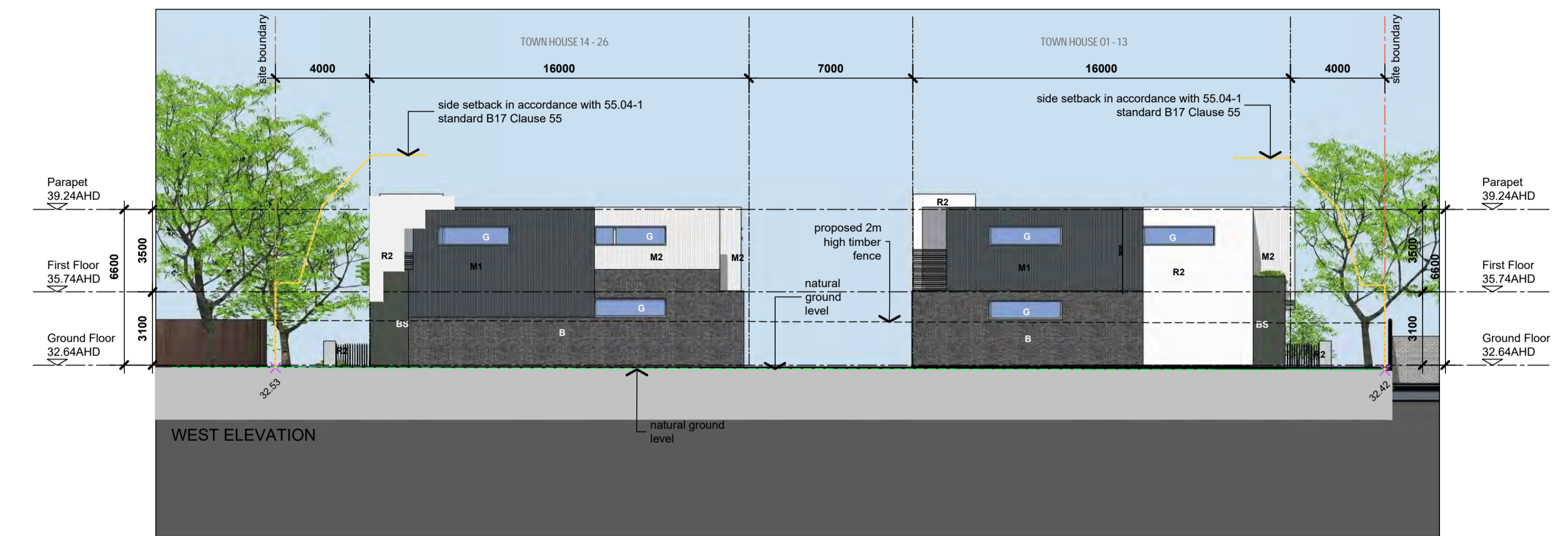
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PRINCES HIGHWAY, PAKENHAM

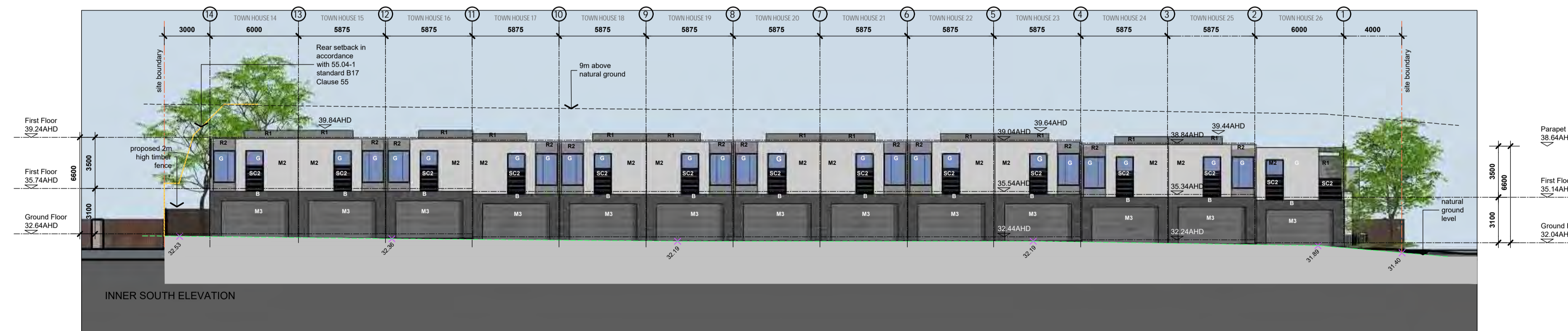


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TP05
REV: 1





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LEGEND

- BS blue stone cladding
- B brick veneer brow
- R1 render finish color gray
- R2 render finish color white
- M1 colorbond cladding color dark
- M2 colorbond cladding color white
- G clear glass panel/windows
- M3 all garage door colorbond finish color gray
- SC overlooking screen color white

REV	DATE	DESCRIPTION
1	03/05/22TP	PRELIMINARY ISSUE

initial	REV	DATE	DESCRIPTION
NH			



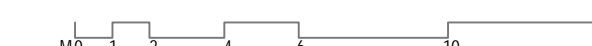
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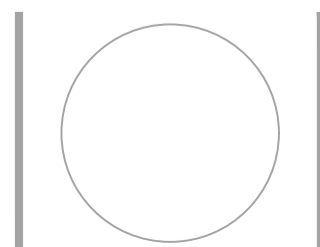
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CLIENT: VALEO CONSTRUCTIONS

DRAWING:
ELEVATIONS

SCALE: 200@A1

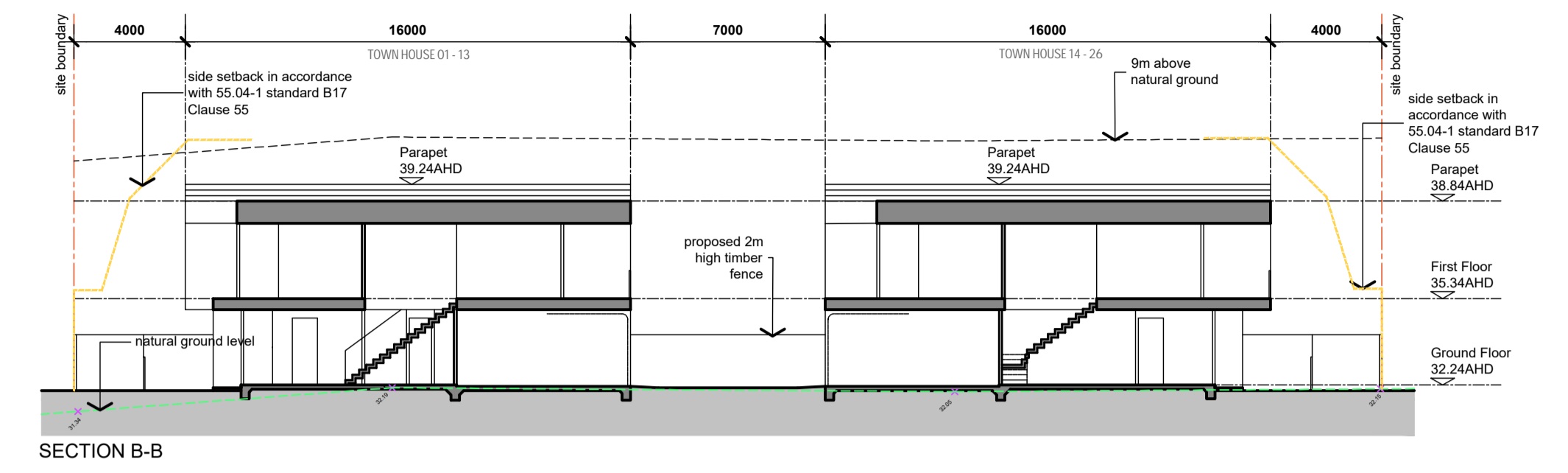
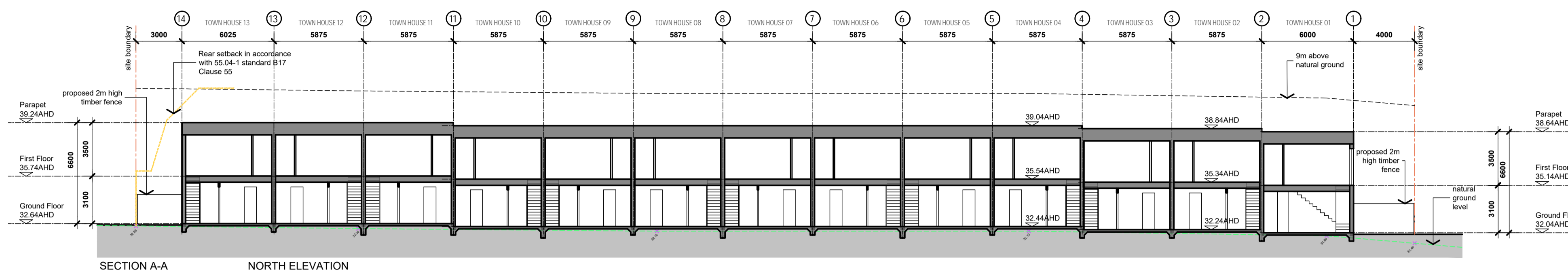


PRINCES HIGHWAY, PAKENHAM



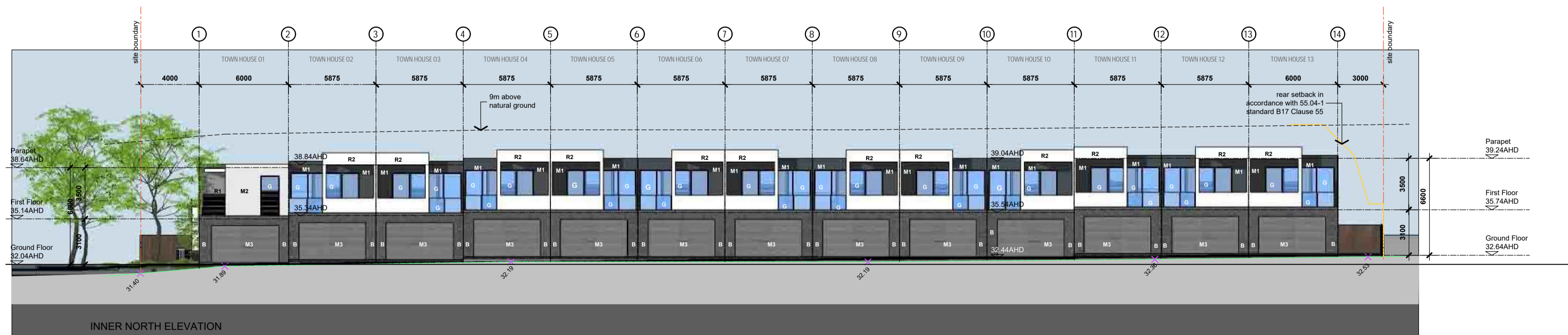
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TPO6
REV: 1

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LEGEND

- BS blue stone cladding
- B brick veneer brow
- R1 render finish color gray
- R2 render finish color white
- M1 colorbond cladding color dark
- M2 colorbond cladding color white
- G clear glass panel/windows
- M3 all garage door colorbond finish color gray
- SC overlooking screen color white



REVISIONS

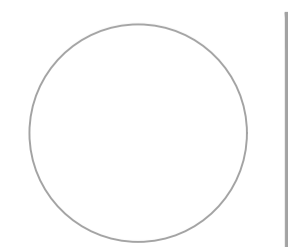
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PROPOSED RESIDENCES
 TOWN PLANNING ISSUE
 3 MAY 2022
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 CLIENT: VALEO CONSTRUCTIONS

DRAWING:
 ELEVATIONS & SECTIONS

SCALE: 200@A1



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 REV: 1

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SIR THOMAS DRIVE

37 Sir Thomas Dr
1 storey
brick house

35 Sir Thomas Dr
1 storey
brick house

33 Sir Thomas Dr
1 storey painted
brick house

31 Sir Thomas Dr
1 storey
brick house

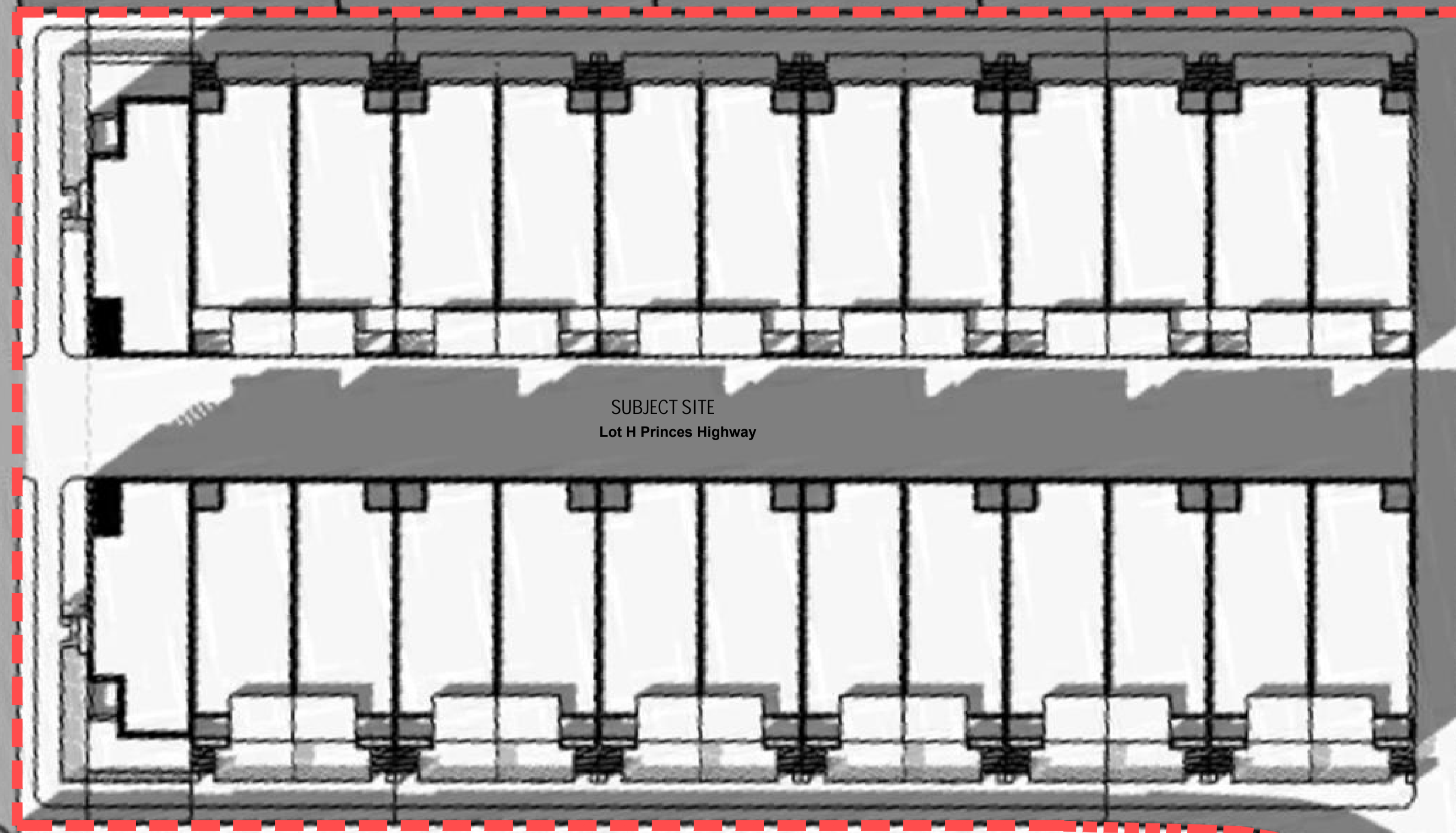
80 Dunbarton Dr
1 storey
brick house

82 Dunbarton Dr
1 storey
brick house

84 Dunbarton Dr
1 storey
brick house

88 Dunbarton Dr
1 storey
brick house

86 Dunbarton Dr
1 storey
brick house



SUBJECT SITE
Lot H Princes Highway

service station and associated convenience shops
town planning approved

PROPOSED EQUINOX - 9AM SHADOW DIAGRAM

REVISIONS

REV	DATE	DESCRIPTION
1	03/05/22TP	PRELIMINARY ISSUE

initial	REV	DATE	DESCRIPTION
NH			



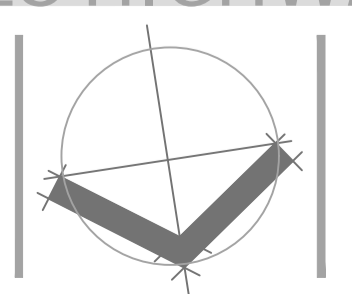
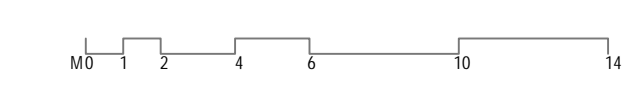
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PROPOSED RESIDENCES

TOWN PLANNING ISSUE
3 MAY 2022
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CLIENT: VALEO CONSTRUCTIONS

DRAWING:
PROPOSED EQUINOX - 9AM SHADOW DIAGRAM

SCALE: 1:200@A1

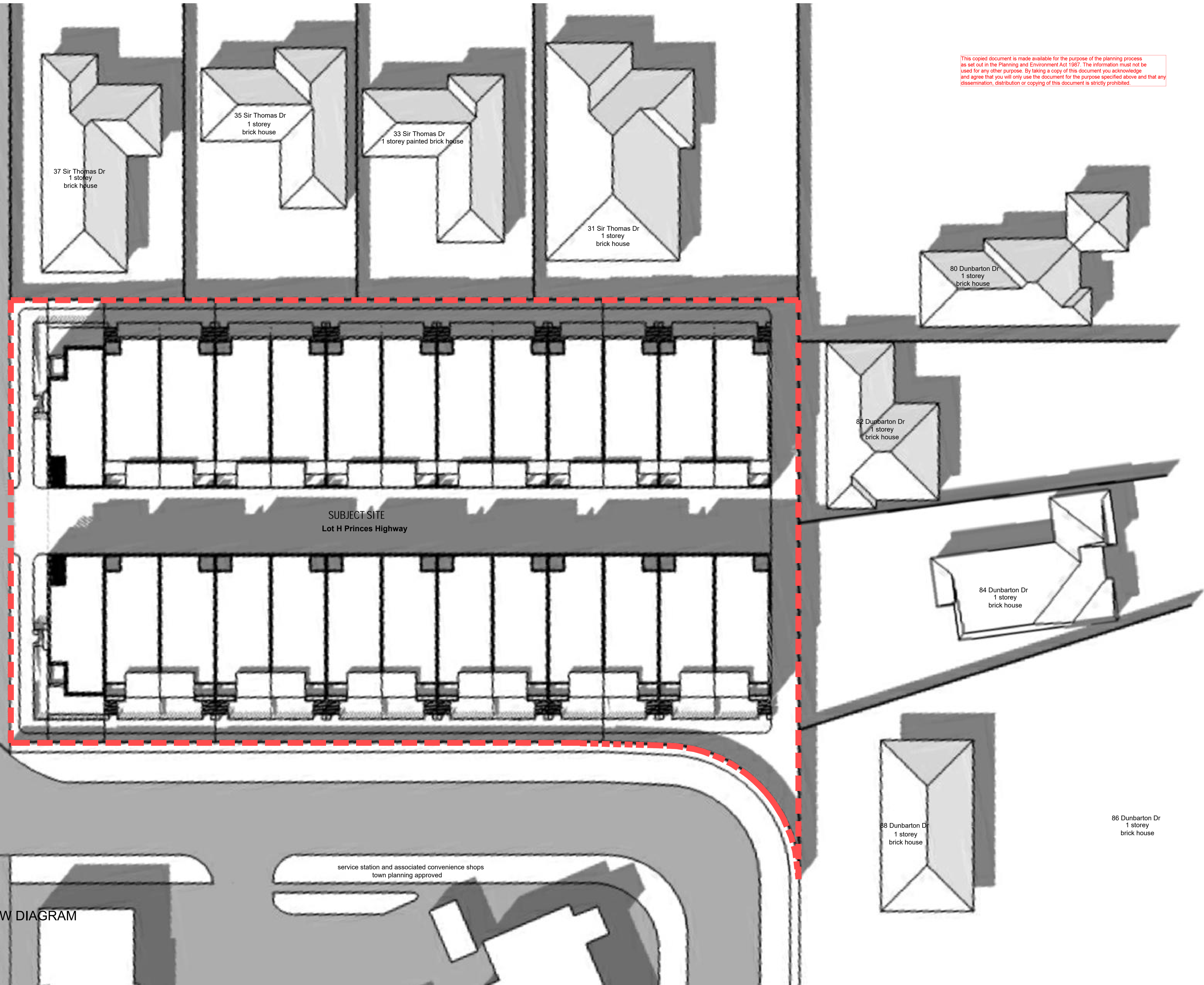


PRINCES HIGHWAY, PAKENHAM

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TP 08
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SIR THOMAS DRIVE



PROPOSED EQUINOX - 10AM SHADOW DIAGRAM

REVISIONS

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VALEO
Shaping TOMORROW

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PROPOSED RESIDENCES
TOWN PLANNING ISSUE
3 MAY 2022
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CLIENT: VALEO CONSTRUCTIONS

DRAWING:
PROPOSED EQUINOX - 10AM SHADOW DIAGRAM
SCALE: 1:200@A1



PRINCES HIGHWAY, PAKENHAM
DRAWG NO:
TP 09
REV: 1

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SIR THOMAS DRIVE

37 Sir Thomas Dr
1 storey
brick house

35 Sir Thomas Dr
1 storey
brick house

33 Sir Thomas Dr
1 storey painted brick house

31 Sir Thomas Dr
1 storey
brick house

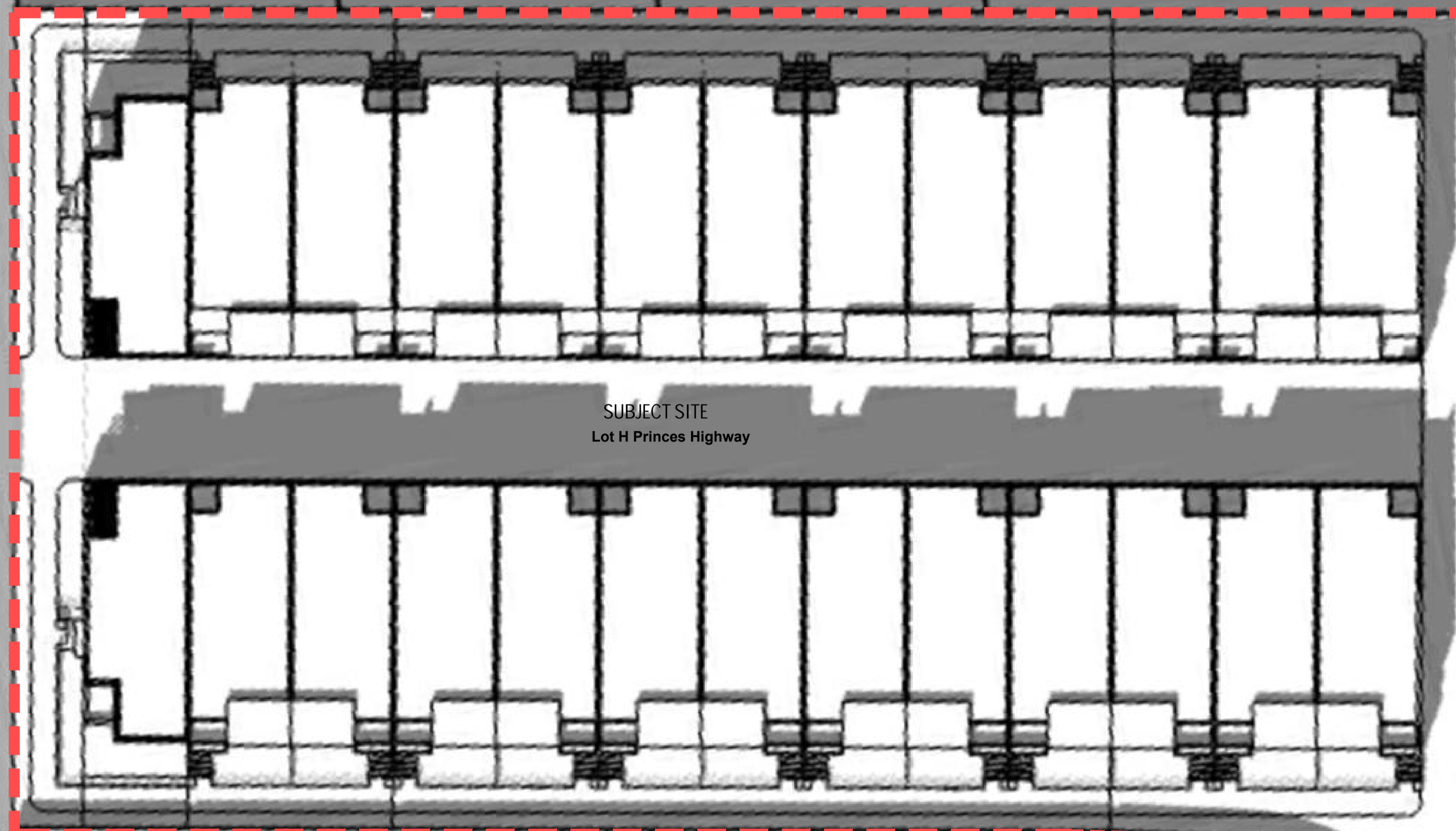
80 Dunbarton Dr
1 storey
brick house

82 Dunbarton Dr
1 storey
brick house

84 Dunbarton Dr
1 storey
brick house

88 Dunbarton Dr
1 storey
brick house

86 Dunbarton Dr
1 storey
brick house



SUBJECT SITE
Lot H Princes Highway

service station and associated convenience shops
town planning approved

PROPOSED EQUINOX - 11AM SHADOW DIAGRAM

REV	DATE	DESCRIPTION
1	03/05/22	TP PRELIMINARY ISSUE

initial	REV	DATE	DESCRIPTION
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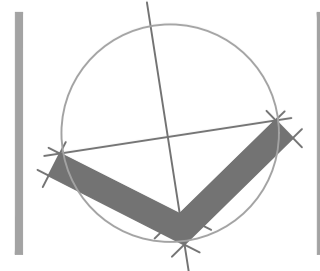
PROPOSED RESIDENCES

TOWN PLANNING ISSUE

3 MAY 2022
DRAWN BY: NH
CLIENT: VALEO CONSTRUCTIONS

DRAWING:
PROPOSED EQUINOX - 11AM SHADOW DIAGRAM

SCALE: 1:200@A1



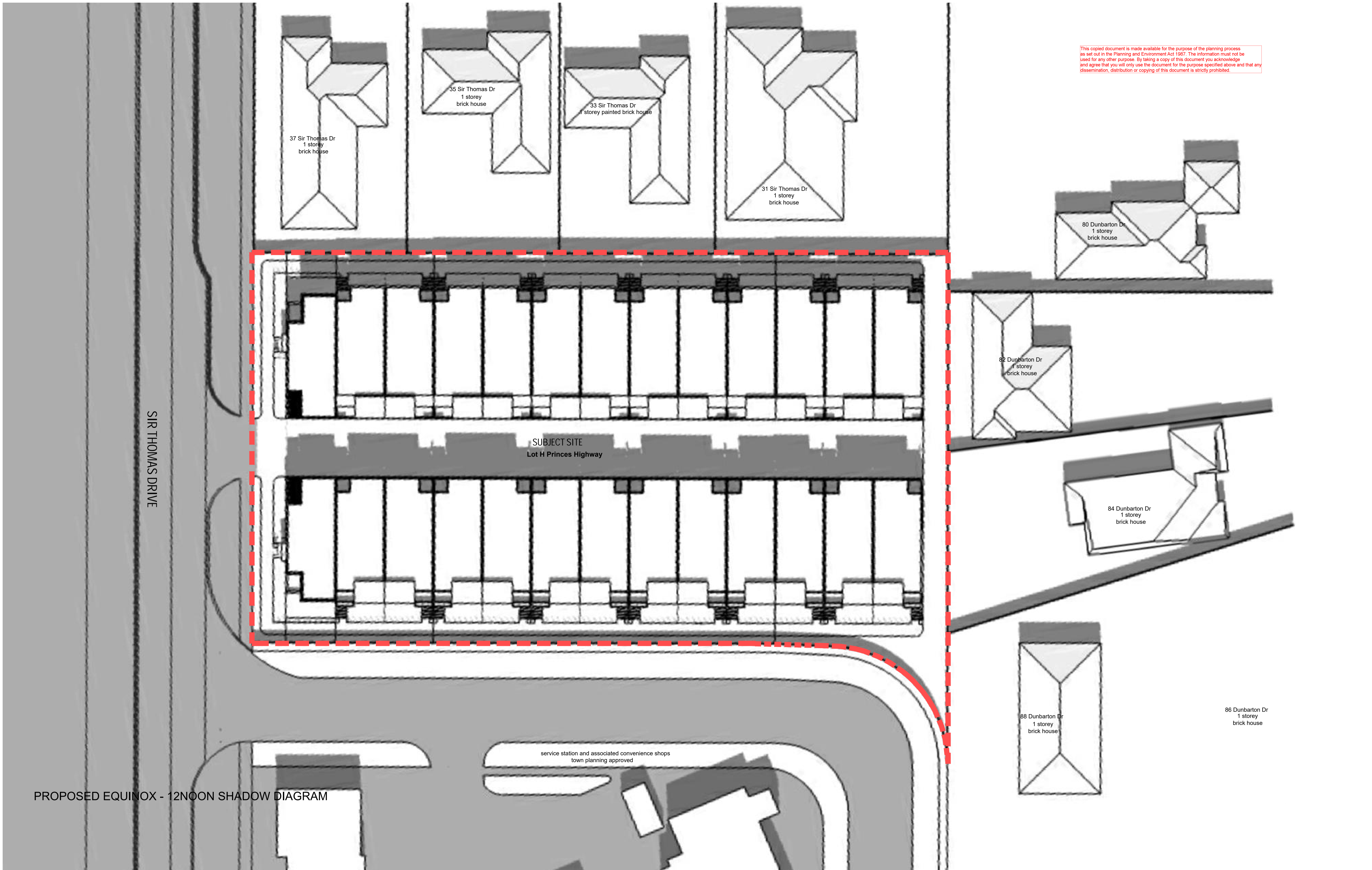
PRINCES HIGHWAY, PAKENHAM

DRWG NO:

TP 10

REV: 1

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SIR THOMAS DRIVE

SUBJECT SITE
Lot H Princes Highway

service station and associated convenience shops
town planning approved

PROPOSED EQUINOX - 12NOON SHADOW DIAGRAM

REVISIONS

REV	DATE	DESCRIPTION
1	03/05/22	TP PRELIMINARY ISSUE

initial	REV	DATE	DESCRIPTION
NH			



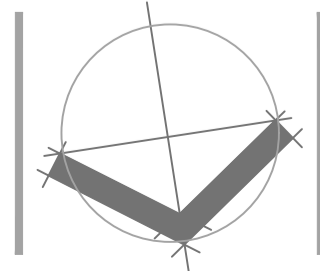
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PROPOSED RESIDENCES

TOWN PLANNING ISSUE
3 MAY 2022
DRAWN BY: NH
CLIENT: VALEO CONSTRUCTIONS

DRAWING:
PROPOSED EQUINOX - 12NOON SHADOW DIAGRAM

SCALE: 1:200@A1



PRINCES HIGHWAY, PAKENHAM

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SIR THOMAS DRIVE

37 Sir Thomas Dr
1 storey
brick house

35 Sir Thomas Dr
1 storey
brick house

33 Sir Thomas Dr
1 storey painted brick house

31 Sir Thomas Dr
1 storey
brick house

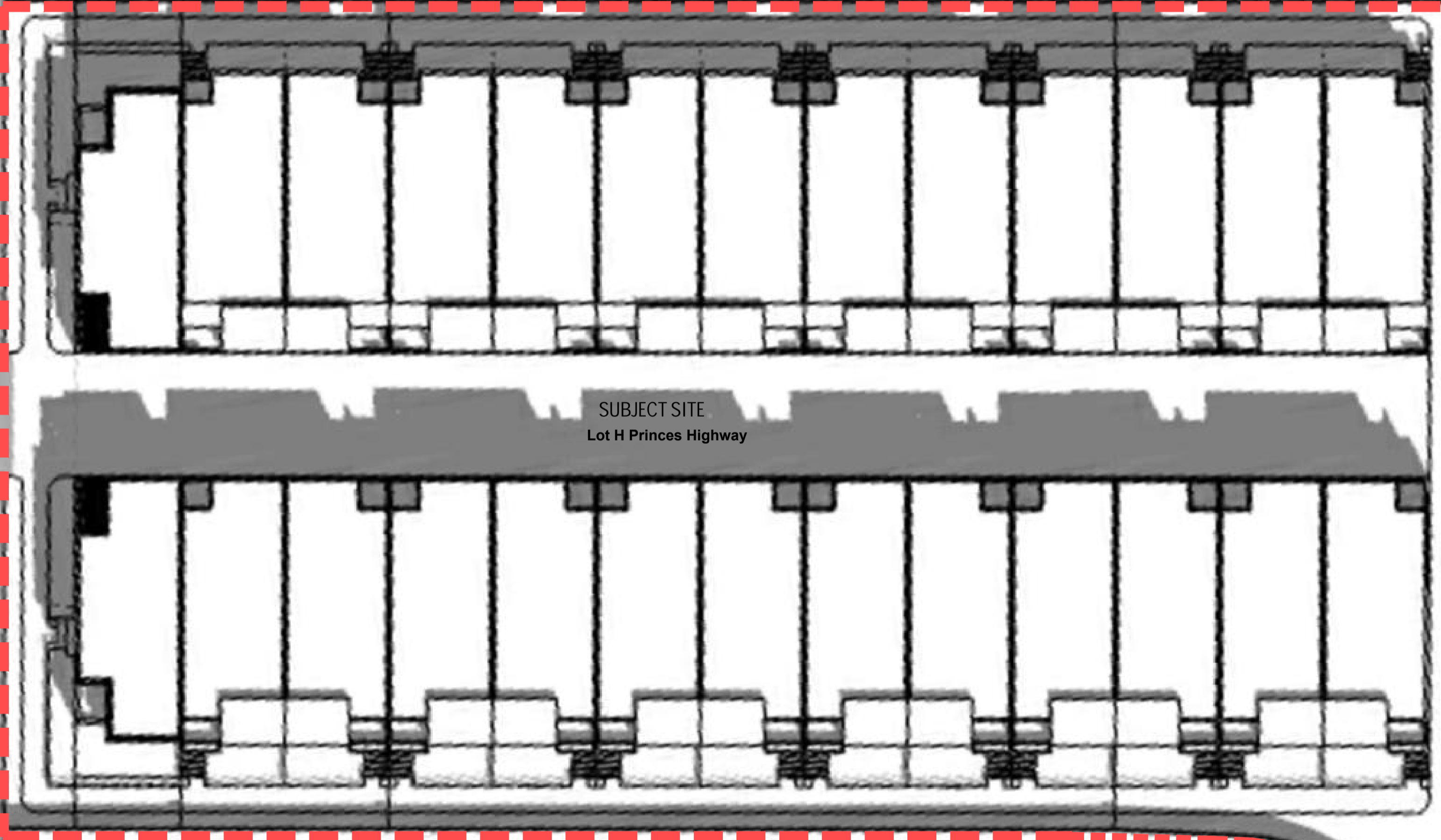
80 Dunbarton Dr
1 storey
brick house

82 Dunbarton Dr
1 storey
brick house

84 Dunbarton Dr
1 storey
brick house

88 Dunbarton Dr
1 storey
brick house

86 Dunbarton Dr
1 storey
brick house



SUBJECT SITE
Lot H Princes Highway

service station and associated convenience shops
town planning approved

PROPOSED EQUINOX - 1PM SHADOW DIAGRAM

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NH			



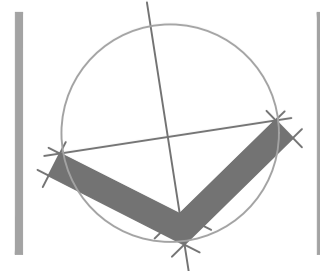
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PROPOSED EQUINOX - 1PM SHADOW DIAGRAM

SCALE: 1:200@A1



PRINCES HIGHWAY, PAKENHAM

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SIR THOMAS DRIVE

37 Sir Thomas Dr
1 storey
brick house

35 Sir Thomas Dr
1 storey
brick house

33 Sir Thomas Dr
1 storey painted brick house

31 Sir Thomas Dr
1 storey
brick house

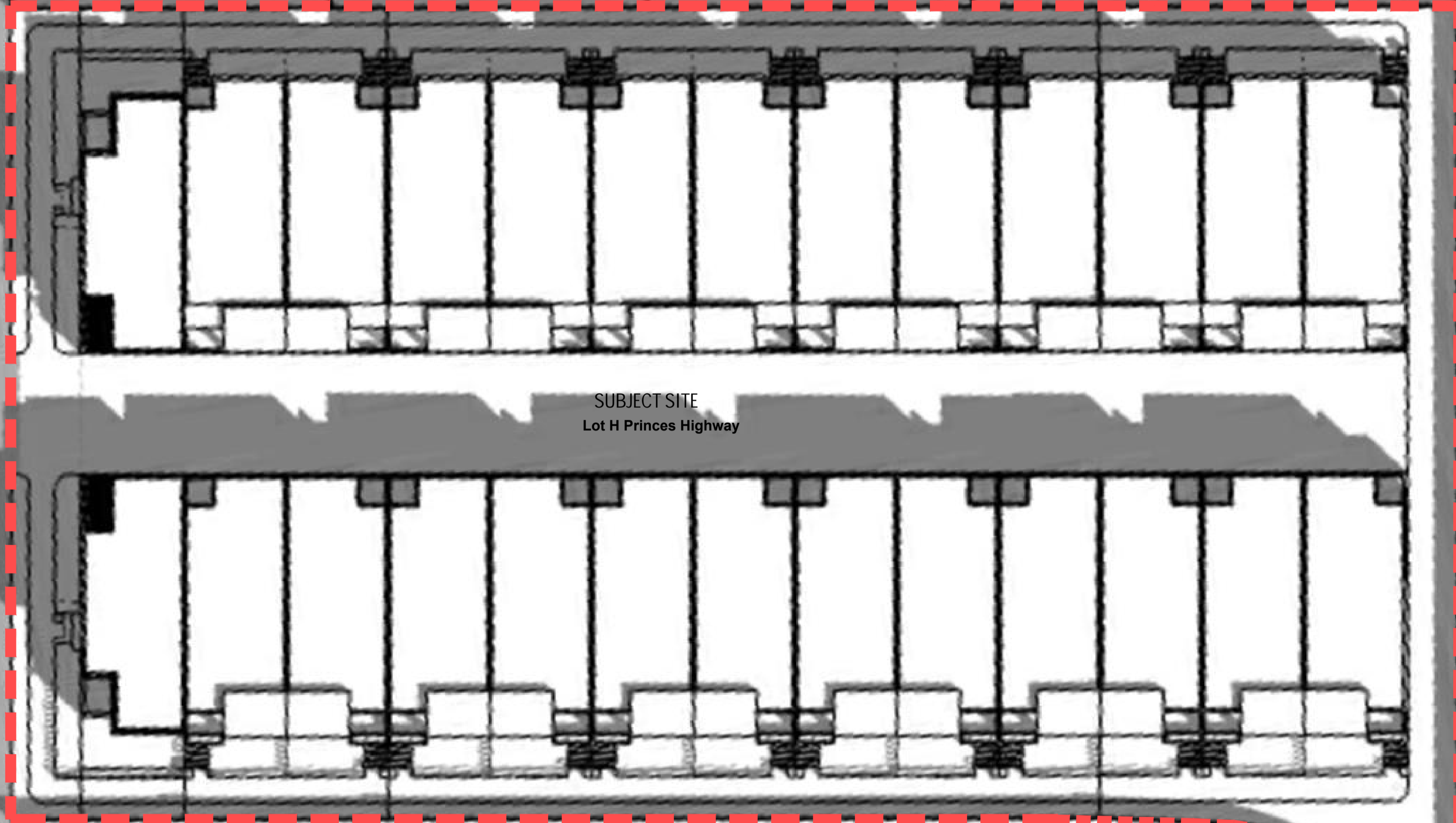
80 Dunbarton Dr
1 storey
brick house

82 Dunbarton Dr
1 storey
brick house

84 Dunbarton Dr
1 storey
brick house

88 Dunbarton Dr
1 storey
brick house

86 Dunbarton Dr
1 storey
brick house



SUBJECT SITE
Lot H Princes Highway

service station and associated convenience shops
town planning approved

PROPOSED EQUINOX - 2PM SHADOW DIAGRAM

REVISIONS

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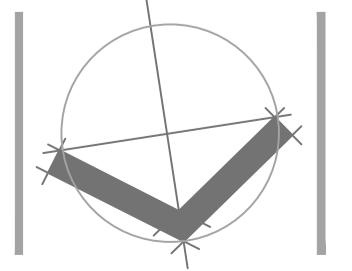
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PROPOSED EQUINOX - 2PM SHADOW DIAGRAM

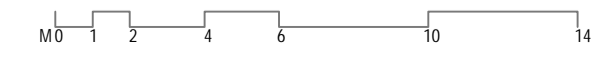
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PRINCES HIGHWAY, PAKENHAM

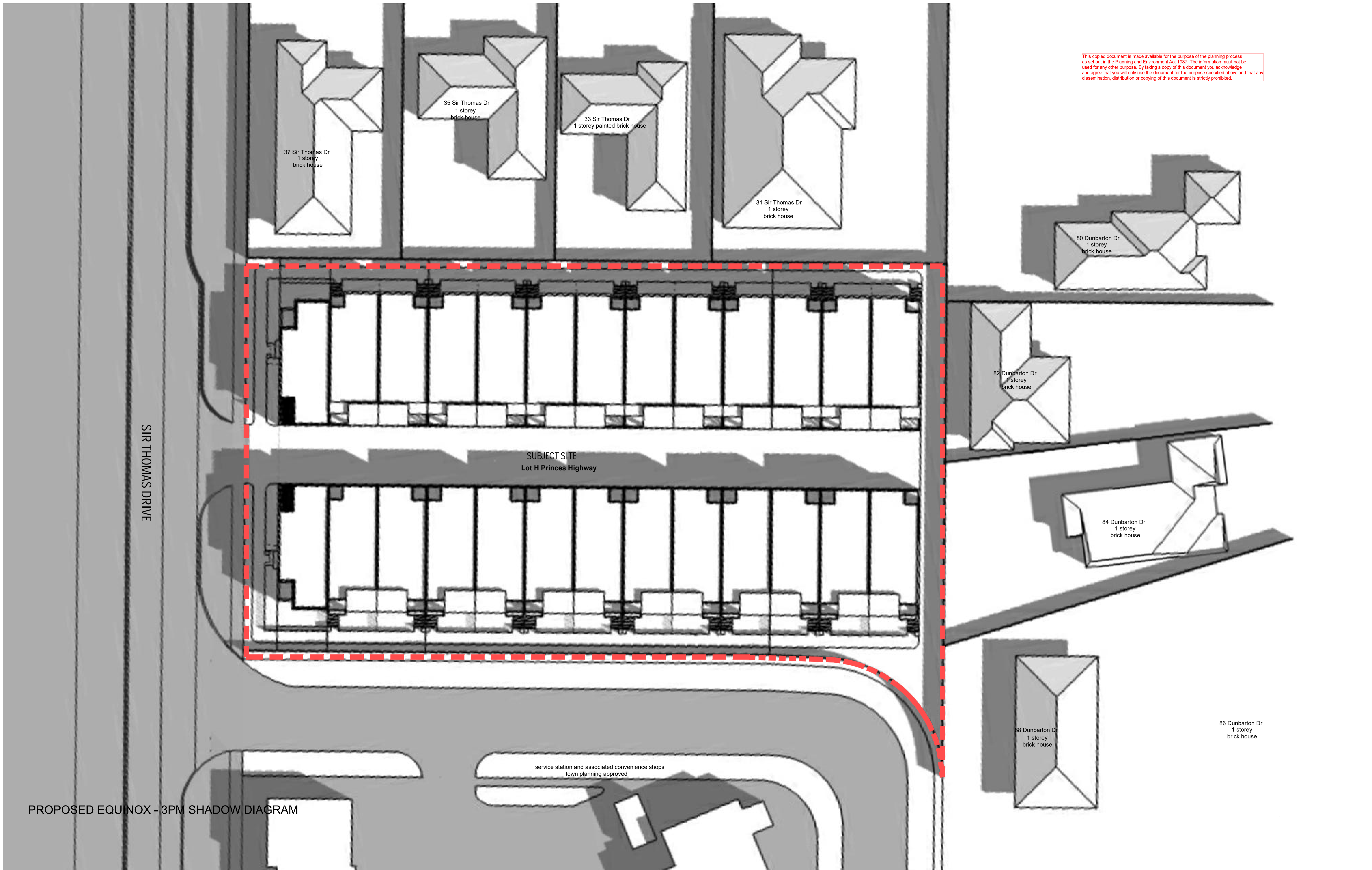


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SIR THOMAS DRIVE

SUBJECT SITE
Lot H Princes Highway

service station and associated convenience shops
town planning approved

PROPOSED EQUINOX - 3PM SHADOW DIAGRAM

REVISIONS

REV	DATE	DESCRIPTION
1	03/05/22TP	PRELIMINARY ISSUE

initial	REV	DATE	DESCRIPTION
NH			



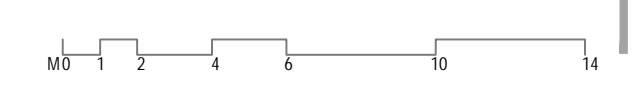
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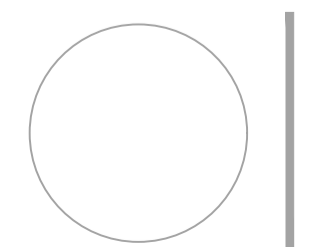
TOWN PLANNING ISSUE
3 MAY 2022
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DRAWING:
PROPOSED EQUINOX - 3PM SHADOW DIAGRAM

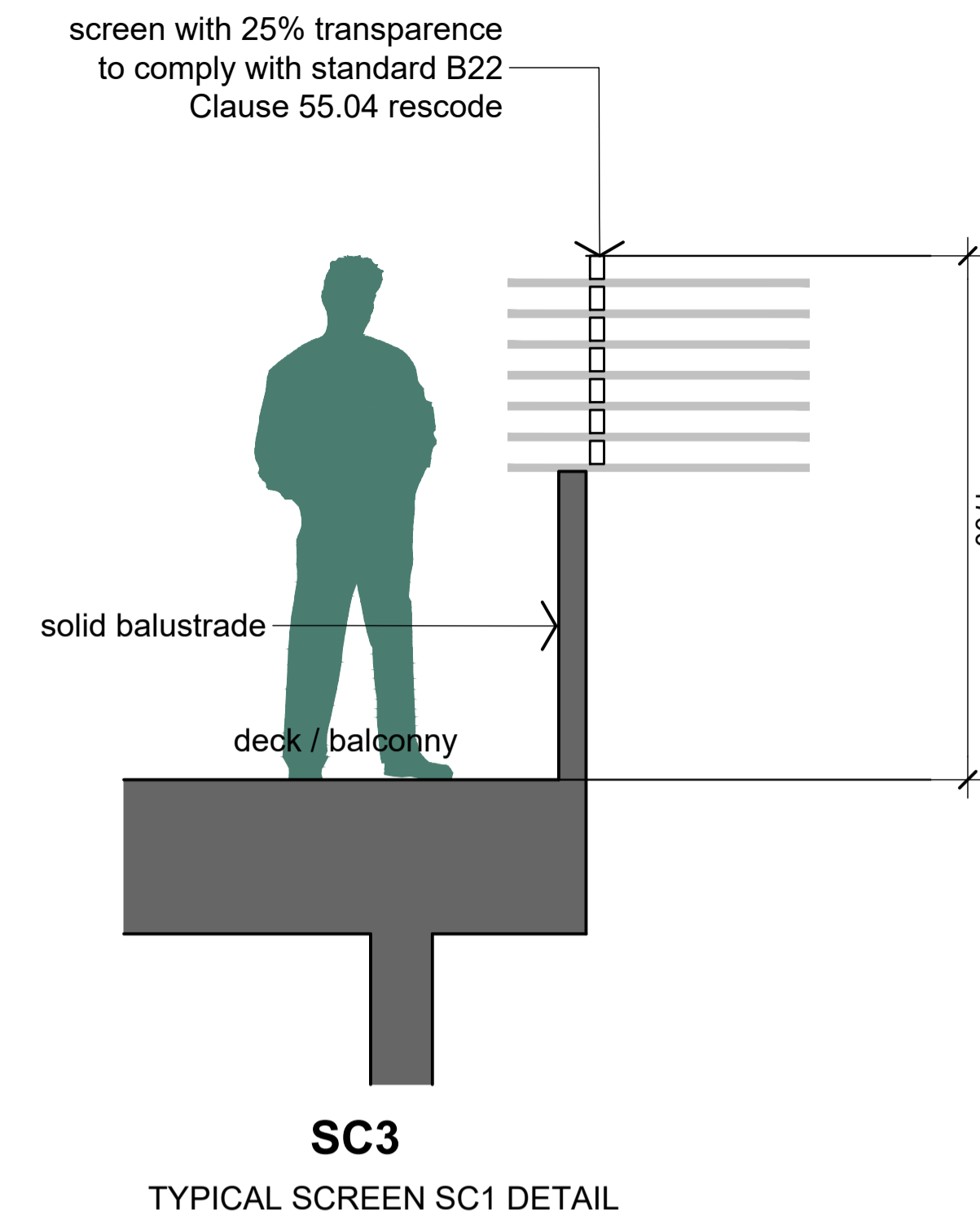
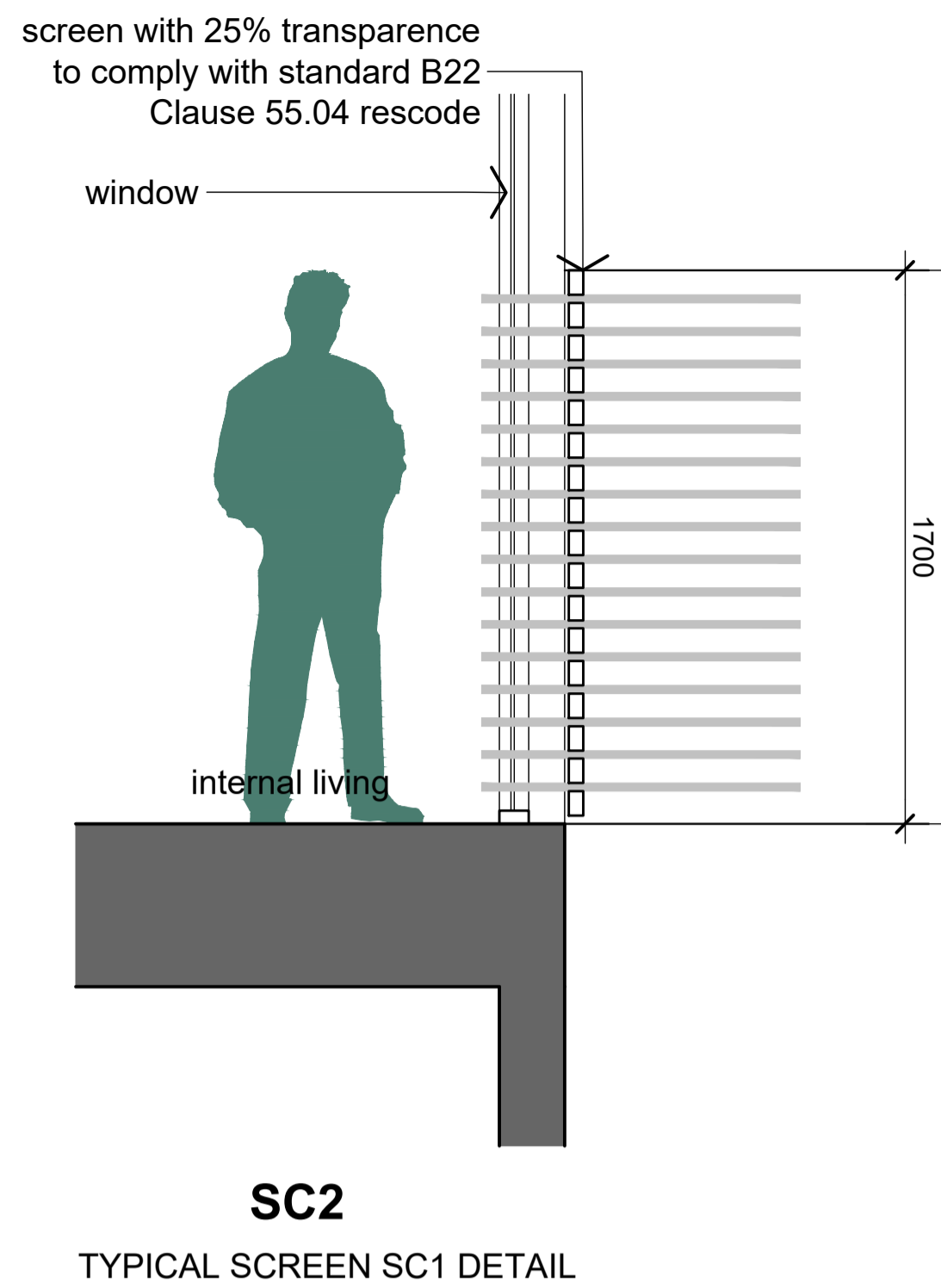
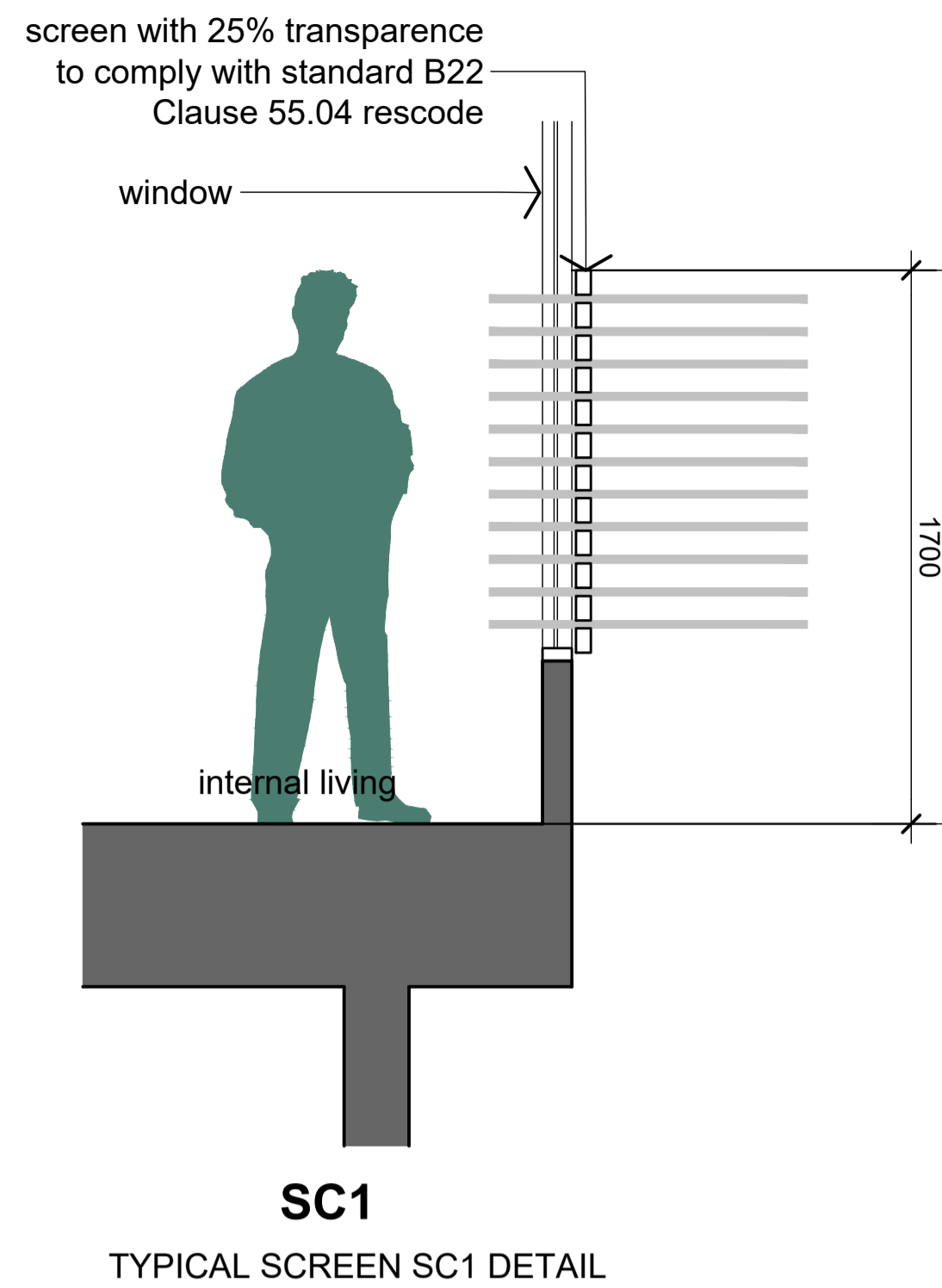
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PRINCES HIGHWAY, PAKENHAM



DRWG NO:
TP 14
REV: 1



SCHEDULE OF MATERIALS, EXTERNAL FINISHES AND CORLORS



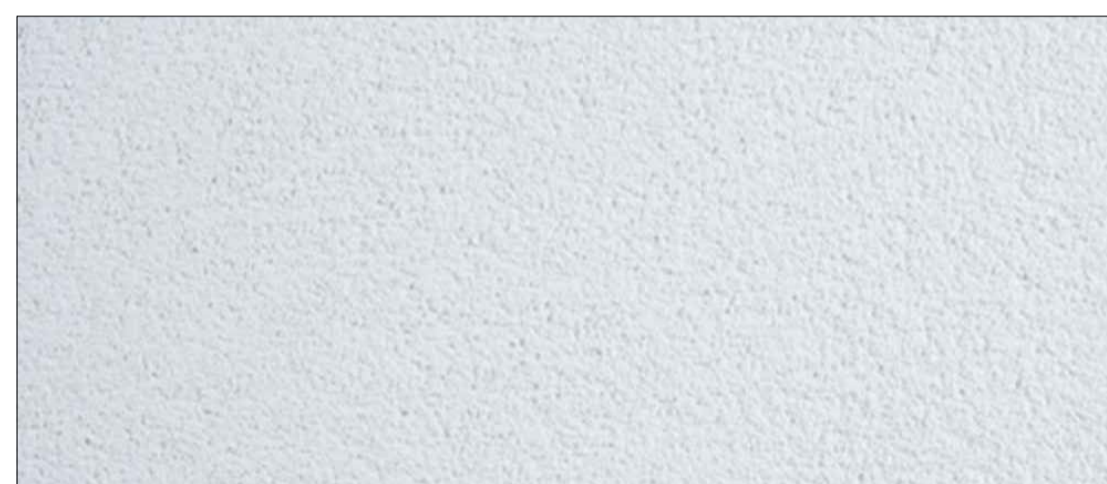
BS blue stone cladding



B brick veneer brown



R1 render finish color gray



R1 render finish color white



M1 colorbond cladding color dark



M2 colorbond cladding color white



G clear glass panel/windows



M3 all garage door colorbond finish color gray



SC overlooking screen color white

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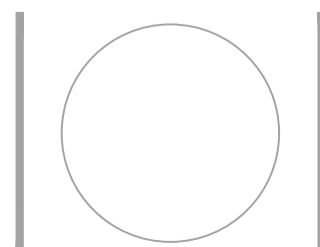
3 MAY 2022
DRAWN BY: NH
CLIENT: VALEO CONSTRUCTIONS

DRAWING:
SCREEN DETAIL & FINISHES SCHEDULE

SCALE: NST@A1



PRINCES HIGHWAY, PAKENHAM



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TP 15

REV: 1



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PROPOSED RESIDENCES

TOWN PLANNING ISSUE

3 MAY 2022
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 CLIENT: VALEO CONSTRUCTIONS

DRAWING:
PERSPECTIVES

SCALE: NST@A1

PRINCES HIGHWAY, PAKENHAM



DRWG NO:
TP 16
 REV: 1